

SPECIFICATIONS**1. Division 1, Advertisement for bids**

- A. Remove and Replace in its entirety.
- B. Changes include:
 - I. Revise completion date of Phase 1 to august 27, 2027.

2. Division 2, Contract Proposal

- A. Remove and Replace in its entirety (Pages 2-3 to 2-20).
- B. Changes include:
 - I. Add Note 5.
 - II. Add Note 6. Clarify the process of awarding contracts based on the combination of lowest multiple prime bidders or one combined single prime bid.
 - III. Add note 7. Clarify who will coordinate the work if multiple prime bids are awarded.
 - IV. Revisions to quantities in Bid Schedule 1 for "6 Concrete Pavement" and "Separation Geotextile".
 - V. Revisions to quantities in Bid Schedule 3 for "6 Concrete Pavement" and "Separation Geotextile".
 - VI. Revision to item description in Bid Schedule 1A for "4" Perforated Polyethylene Underdrain Pipe - w/Sock, Complete"
 - VII. Revision to item description in Bid Schedule 1C for "4" Perforated Polyethylene Underdrain Pipe - w/Sock, Complete".
 - VIII. Renumber Notes 5 through 13 to Notes 8 through 16.
 - IX. Revised Note 11 (originally Note 8) to clarify the specified liquidated damages amount.

3. Division 3, Contract Agreement

- A. Remove and Replace in its entirety (Pages 3-1 to 3-2).
- B. Changes include:
 - I. Revised Time of Performance section to clarify the specified liquidated damages amount and completion date.

4. Division 4, FAA General Contract Provisions: 80-08 – Execution and Progress

- A. Remove and Replace in its entirety (Pages GP-35 to GP-40).
- B. Changes include:
 - I. Revised completion date of Phase 1.
 - II. Revised Liquidated Damages Cost of Phase 1 and Phase 2.
 - III. Added table of Liquidated Damages based on Contract Amount.

5. D-705 Pipe Underdrain for Airport

- A. Revise section 705.5.1 Pipe Underdrains, Complete, 4th paragraph.
- B. Changes include:
 - I. Remove "Schedule 40" from the pay item.
- C. Clarification:
 - I. Underdrain pipe shall be perforated polyethylene pipe.

PLANS**1. Sheet G-081**

- A. Remove and Replace in its entirety.
- B. Changes include:
 - i. Revise Note 11 to: "THE CONCURRENT TERMINAL BUILDING CONTRACTOR HAS INSTALLED A TEMPORARY 10' CHAINLINK CONSTRUCTION FENCE AROUND THEIR WORK AREA, SHOWN ON THIS SHEET AS THE CONCURRENT TERMINAL BUILDING CONSTRUCTION FENCE. THE CONTRACTOR SHALL ACCESS SITE THROUGH AIRPORT GATE 20 AND A TEMPORARY GATE INSTALLED IN TEMPORARY CONSTRUCTION FENCE INSTALLED BY THE CONTRACTOR. WHEN THE TEMPORARY GATE IS TO REMAIN OPEN A GATE GUARD MUST BE PRESENT AT ALL TIMES. WHEN THE GATE IS NOT IN USE OR THE CONTRACTOR IS NOT ON SITE THE GATE SHALL BE LOCKED".
 - ii. Change Note 12 to Note 13
 - iii. Add new Note 12 to "ONCE THE CONCURRENT TERMINAL BUILDING CONTRACTOR CONSTRUCTION FENCE IS REMOVED AND THE PROPOSED PERMENENT SECURITY AND WILDLIFE FENCES ARE NOT IN PLACE THERE SHALL BE A GATE GUARD PRESENT AT AIRPORT GATE 20. THE CONTRACTOR SHALL COORDINATE WITH THE TERMINAL BUILDING CONTRACTOR ON WHO WILL BE RESPONSIBLE ON PROVIDING A GATE GUARD ON A GIVEN DAY."

2. Sheet G-082

- A. Remove and Replace in its entirety.
- B. Changes include:
 - i. Revise Note 2 to: "PHASE 1 SHALL BE COMPLETED BY AUGUST 27, 2027. CONTRACTOR MAY START CONSTRUCTION IN THE FALL OF 2026 ONCE THE NOTICE TO PROCEED HAS BEEN GIVEN BY THE OWNER."

3. Sheet G-083

- A. Remove and Replace in its entirety.
- B. Changes include:
 - i. Revise Note 2 to: "THE CONTRACTOR TO COORDINATE WITH ENGINEER, THE AIRPORT, AND CONCURRENT TERMINAL BUILDING CONTRACTOR FOR REMOVAL/RELOCATION OF THE CONCURRENT TERMINAL BUILDING CONSTRUCTION FENCE FOR APRON CONSTRUCTION. AIRPORT PERIMETER FENCE TO BE MAINTAINED AT ALL TIMES. GATE GUARD AT EXISTING GATE 20 TO BE USED IF AIRPORT PERIMETER FENCE IS NOT CONTINUOUS."

4. Sheet C-304

- A. Remove and Replace in its entirety.
- B. Changes include:
 - i. Revise Detail 1 "Proposed Compacted Millings Perimeter Road Typical Section"
- C. Clarification:
 - i. Revised detail shows embankment under the proposed compacted millings perimeter road.

5. Sheets C-321 Jointing Plan - Base Bid

- A. Remove and Replace in its entirety.
- B. Changes include:
 - i. Revised leader callouts to the correct typical sections for Concrete Perimeter Roads and Tug Bay Pad

6. C-323 Jointing Plan – Bid Alternate 1

- A. Remove and Replace in its entirety.
- B. Changes include:
 - i. Revised leader callouts to the correct typical sections for Concrete Perimeter Roads and Tug Bay Pad.

7. C-451 Drainage Details

- A. Changes include:
 - i. Detail 1 RCP STORM SEWER TRENCH DETAIL - Revised compaction percentage backfill material to 95% of the modified density.

8. C-453 Drainage Details

- A. Changes include:
 - i. Detail 3 VALVE-1 24" GATE VALVE WITH BOX – Delete reference to SDR-35 PVC pipe.

9. E-302 Apron Power Design

- A. Add the following notes:
 - i. Note 2: Contractor to extend conduit between Panel 1PNH4 to Lighting Control Panel (LCP) located in Room 152 for apron lighting. Contractor to provide additional relay to LCP as needed for control of the apron lighting. Coordinate with Terminal Contractor.
 - ii. Note 3: Contractor to provide new 277V, 1P, 30A breaker for apron lighting circuit in Panel 1PNH4.
 - iii. Note 4: Contractor to provide new 208V, 3P, 50A breaker to Panel 1PNL4 in Room 123 for truck deicing panel.
- B. Make the following change:
 - i. Change panel labeling in Room 123 from 1PNL2 to 1PNL4.

ATTACHMENTS

- Revised Ad for Bid (Specification Pages 1-1 to 1-4)
- Revised Contract Proposal and Bid Schedule (Specification Pages 2-3 to 2-20)
- Revised Contract Agreement (Specification Pages 3-1 to 3-3)
- FAA General Contract Provisions Section 80 (Specification Pages GP-35 to GP-40)
- Sheet G-081R
- Sheet G-082R
- Sheet G-083R
- Sheet C-304R
- Sheet C-321R
- Sheet C-323R
- Bidder Questions Answered

END OF ADDENDUM No. 1



DIVISION 1

ADVERTISEMENT FOR BIDS

Dickinson Theodore Roosevelt Regional Airport

Dickinson, North Dakota

AIP Project No. 3-38-0013-055-2026

Project Name: Terminal Apron & Taxiways H & J Construction

Bid Date: Thursday, June 11, 2026 at 11:00 AM Local Time

Sealed bids, subject to the conditions contained herein, for improvements to the Dickinson Theodore Roosevelt Regional Airport, Dickinson, North Dakota, AIP Project No. 3-38-0013-055-2026 will be received by the Dickinson Municipal Airport Authority, 11120 42nd Street SW, Dickinson, North Dakota, 58601, until 11:00 AM local time on June 11, 2026, and then publicly opened and read aloud.

Project work consists of but is not limited to the following:

Terminal Apron & Taxiway H & J Construction will consist of excavation, earthwork, grading, storm sewer installation, aggregate base, concrete paving, fencing, seeding, and electrical work.

Construction for this project shall be completed by: August 27, 2027 for all Phases.

Contract Documents. The complete set of Plans, Specifications, and Contract Documents can be downloaded for a fee of \$25 at www.questcdn.com #10209350. Those wishing to download the bidding documents electronically, do so at their own risk for completeness of the bidding documents.

Bidding. Each bid must be accompanied by a separate envelope containing the contractor's license and bid security (if e-bids required, the e-bid process will be set up to meet this requirement). The bid security must be in a sum equal to five percent of the full amount of the bid and must be in the form of a bidder's bond. A bidder's bond must be executed by the bidder as principal and by a surety, conditioned that if the principal's bid is accepted and the contract awarded to the principal, the principal, within ten days after notice of the award, shall execute a contract in accordance with the terms of the bid and the bid bond and any condition of the governing body. If a successful bidder does not execute a contract within the ten days allowed, the bidder's bond must be forfeited to the governing body and the project awarded to the next lowest responsible bidder.

The airport reserves the right to reject any and all bids and rebid the project. No bid will be read or considered if the bid does not fully comply with the bidding requirements. Deficient bids will be resealed and returned to the bidder.

Multiple Prime Bids for General, Electrical, and Mechanical. The Owner will receive multiple prime bids for general and electrical construction or any combination thereof per century code 48-01.206 bid requirements for public improvements, multiple prime bids for general, mechanical, and electrical.

Federal Requirements for Federally Funded Projects. This Project is being partially funded under the Federal Aviation Administration (FAA) Airport Improvement Program (AIP). Contractors must comply with specific federally required provisions as listed herein and contained in the contract documents. The following federal provisions are incorporated in this solicitation by reference:

1. Buy American Preference (49 USC § 50101; Executive Order 14005; Infrastructure Investment and Jobs Act (IIJA) (P. L. No. 117-58); Build America; Buy America (BABA))
2. Civil Rights – Title VI Assurance (49 USC § 47123; FAA Order 1400.11)
3. Davis-Bacon Act (2 CFR Part 200, Appendix II (D); 29 CFR Part 5; 49 USC § 47112 (b); 40 USC § 3141-3144, 3146, and 3147)
4. Debarment and Suspension (2 CFR Part 180 (Subpart B); 2 CFR Part 200, Appendix II(H); 2 CFR Part 1200; DOT Order 4200.5; Executive Orders 12549 and 12689)
5. Disadvantaged Business Enterprise (49 CFR Part 26; 49 USC § 47113)
6. Federal Fair Labor Standards Act (29 USC § 201, et seq; 2 CFR § 200.430)
7. Foreign Trade Restriction (49 CFR Part 30; 49 USC § 50104)
8. Lobbying and Influencing Federal Employees (49 CFR Part 20, Appendix A; 31 USC § 1352 – Byrd Anti-Lobbying Amendment; 2 CFR part 200- Appendix II(I))
9. Procurement of Recovered Materials (2 CFR § 200.323; 2 CFR Part 200, Appendix II (J); 40 CFR Part 247; 42 USC § 6901, et seq (Resource Conservation and Recovery Act (RCRA)))
10. Prohibition of Covered Unmanned Aircraft Systems (UAS) (FAA Reauthorization Act of 2024(Public Law 118-63), Section 936; 49 USC § 44801)

Disadvantaged Business Enterprise. The requirements of 49 CFR Part 26 including any amendments thereto apply to this contract. In accordance with the amendment dated October 3, 2025, no DBE goals have been established for this contract. However, the contract clauses in 49 CFR §§ 23.9, 26.13(a-b), and 26.29 still apply to this contract. It is the policy of the Dickinson Theodore Roosevelt Regional Airport to practice nondiscrimination based on race, color, sex, or national origin in the award or performance of this contract. The Owner encourages participation by all firms qualifying under this solicitation regardless of business size or ownership.

Federal Fair Labor Standards Act. All contracts and subcontracts that result from this solicitation incorporate by reference the provisions of 29 CFR part 201, et seq, the Federal Fair Labor Standards Act (FLSA), with the same force and effect as if given in full text. The FLSA sets minimum wage, overtime pay, recordkeeping, and child labor standards for full and part-time workers.

The Contractor has full responsibility to monitor compliance to the referenced statute or regulation. The Contractor must address any claims or disputes that arise from this requirement directly with the U.S. Department of Labor – Wage and Hour Division.

Trade Restriction Certification. By submission of an offer, the Offeror certifies that with respect to this solicitation and any resultant contract, the Offeror -

1. is not owned or controlled by one or more citizens of a foreign country included in the list of countries that discriminate against U.S. firms as published by the Office of the United States Trade Representative (U.S.T.R.);
2. has not knowingly entered into any contract or subcontract for this project with a person that is a citizen or national of a foreign country included on the list of countries that discriminate against U.S. firms as published by the USTR; and
3. has not entered into any subcontract for any product to be used on the Federal project that is produced in a foreign country included on the list of countries that discriminate against U.S. firms published by the USTR.

This certification concerns a matter within the jurisdiction of an agency of the United States of America and the making of a false, fictitious, or fraudulent certification may render the maker subject to prosecution under Title 18 USC § 1001.

The Offeror/Contractor must provide immediate written notice to the Owner if the Offeror/Contractor learns that its certification or that of a subcontractor was erroneous when submitted or has become erroneous by reason of changed circumstances. The Contractor must require subcontractors provide immediate written notice to the Contractor if at any time it learns that its certification was erroneous by reason of changed circumstances.

Unless the restrictions of this clause are waived by the Secretary of Transportation in accordance with 49 CFR § 30.17, no contract shall be awarded to an Offeror or subcontractor:

1. who is owned or controlled by one or more citizens or nationals of a foreign country included on the list of countries that discriminate against U.S. firms published by the U.S.T.R. or
2. whose subcontractors are owned or controlled by one or more citizens or nationals of a foreign country on such U.S.T.R. list; or
3. who incorporates in the public works project any product of a foreign country on such U.S.T.R. list.

Nothing contained in the foregoing shall be construed to require establishment of a system of records in order to render, in good faith, the certification required by this provision. The knowledge and information of a contractor is not required to exceed that which is normally possessed by a prudent person in the ordinary course of business dealings.

The Offeror agrees that, if awarded a contract resulting from this solicitation, it will incorporate this provision for certification without modification in all lower tier subcontracts. The Contractor may rely on the certification of a prospective subcontractor that it is not a firm from a foreign country included on the list of countries that discriminate against U.S. firms as published by USTR, unless the Offeror has knowledge that the certification is erroneous.

This certification is a material representation of fact upon which reliance was placed when making an award. If it is later determined that the Contractor or subcontractor knowingly rendered an erroneous certification, the Federal Aviation Administration (FAA) may direct through the Owner cancellation of the contract or subcontract for default at no cost to the Owner or the FAA.

Civil Rights - Title VI Assurances. The Dickinson Municipal Airport Authority in accordance with the provisions of Title VI of the Civil Rights Act of 1964 (78 Stat. 252, 42 U.S.C. §§ 2000d to 2000d-4), 28 CFR § 50.3, and 49 CFR Part 21, hereby notifies all bidders that it will affirmatively ensure that any contract entered into pursuant to this advertisement, all contractors will be afforded full opportunity to submit bids in response to this invitation and will not be discriminated against on the grounds of the owner’s race, color, national origin, sex, creed, age, or disability in consideration for an award.

Prohibition of Covered Unmanned Aircraft Systems (UAS). The Bidder or Offeror certifies that they are aware of and comply with relevant Federal statutes and regulations, including those from the Federal Aviation Administration (FAA), for operating unmanned aircraft systems (UAS) in accordance, and in compliance with all related requirements in the FAA Reauthorization Act of 2024 (Public Law 118-63), section 936 (49 U.S.C. § 44801 note).

Contractor warrants that all UAS operations will be conducted in full compliance with all applicable Federal Aviation Administration (FAA) regulations, including but not limited to 14 CFR Part 107, and any other applicable local, state, or Federal laws and regulations.

Sponsors and subgrant recipients cannot use AIP grant funds to enter into, extend, or renew a contract related to covered unmanned aircraft systems (UAS). This includes both procurement and operational contracts, as well as contracts with entities that operate such systems

Federal Funding. The award of the contract is subject to approval of the Federal Aviation Administration and availability of federal funding. See instruction to bidders for additional information.

Any questions regarding bids are to be directed to:

Mead & Hunt, Inc.

Address: 1905 North Plaza Boulevard - Suite 2, Rapid City, SD 57702

Phone: 605-610-2938

Advertised on: May 20, 2026, May 27, 2026, and June 3, 2026

CONTRACT PROPOSAL

TO: Dickinson Municipal Airport Authority
 Dickinson Theodore Roosevelt Regional Airport

PROJECT LOCATION: Dickinson, North Dakota

PROJECT DESCRIPTION: Terminal Apron & Taxiways H & J Construction
 FAA AIP Project No.: 3-38-0013-055-2026
 Mead & Hunt Project No.: 2796100-221238.01

BID OPENING: Dickinson Theodore Roosevelt Regional Airport ARFF Building Conference Room at 11120 42nd Street SW, Dickinson, North Dakota, 58601, Thursday, June 11, 2026, at 11:00 AM local time

BIDDER: _____

BIDDER'S: _____

ADDRESS _____

DATE: _____

BIDDER'S REPRESENTATIVE (to be contacted for additional information on this Proposal):

(Name)

(Telephone Number)

BIDDER'S DECLARATION AND UNDERSTANDING

- The undersigned hereby certifies that they have examined the form of contract, plans and specifications and other associated Contract Documents for the improvement of Dickinson Theodore Roosevelt Regional Airport, AIP Project No. 3-38-0013-055-2026. The undersigned further certifies that he/she has examined the site of the work, has determined for himself/herself the conditions affecting the work and subject to acceptance of the proposal, agrees to provide at his/her expense, all labor, insurance, superintendence, machinery, plant, equipment, tools, apparatus, appliances, and means of construction, and all materials and supplies complete the entire work, including work incidental thereto, in conformance with the plans, specifications, and associated contract documents.

2. The undersigned acknowledges that the Contract Documents consist of the Advertisement for Bid, Instruction to Bidders, all proposal forms contained under Division 2, all issued Addenda, all Contract Forms contained under Division 3, FAA General Provisions, Special Provisions, Construction Safety and Phasing Plan (CSPP), Wage Rates, Technical Specifications, attached appendices, referenced documents and Plans.
3. The undersigned, in compliance with your Advertisement for Bids dated May 20, 2026, hereby proposes to do the work called for in said contract and specifications and shown on said plans and to furnish all materials, tools, labor, and all appliances and appurtenances necessary for the said work at the rates and prices shown on the bid form.
4. The undersigned understands that the above quantities of work to be done are approximate only and are intended principally to serve as a guide in evaluating the bids.
5. The Owner intends to award a contract resulting from this solicitation to the lowest, responsive, responsible bidder, whose offer, conforming to the solicitation, will be the most advantageous to, and in the best interest of the Owner, cost or price and other factors considered.
6. Base Bid Only: Award will be based on the combination of the lowest multiple prime bidders for Bid Schedules 1 and 2 or the lowest single prime bidder for Bid Schedule 3, if the single prime bidder is lower than the combination of the lowest multiple prime bidders for Bid Schedules 1 and 2.

Base Bid + Bid Alternate 1: Award will be based on the combination of the lowest multiple prime bidders for Bid Schedule 1 (including Bid Alternate 1A) and Bid Schedule 2 (including Bid Alternate 1B) or the lowest single bidder for Bid Schedule 3 (including Bid Alternate 1C), if the lowest single prime bidder is lower than the combination of the lowest multiple prime bidders for Bid Schedules 1 and 2 (including bid alternates).

Award of the Bid Alternate will be dependent upon bids received and available funding. It is the intent of the Owner to award one of the following combinations of bid schedules

Base Bid Only

- Multiple Prime Bidders: Bid Schedules 1 & 2
- Single Prime Bidder: Bid Schedule 3

Base Bid + Bid Alternate

- Multiple Prime Bidders: Bid Schedule 1 (including Bid Alternate 1A) & Bid Schedule 2 (including Bid Alternate 1B)
- Single Prime Bidder: Bid Schedule 3 (including Bid Alternate 1C)

7. If the project is awarded as multiple prime contracts for the general and electrical construction, the owner will assign the coordination of the electrical contract and any other contracts to the general/civil contractor for the project to facilitate the coordination of the work.

BID SCHEDULE**Airport: Dickinson - Theodore Roosevelt Regional Airport****FAA AIP Project No. 3-38-0013-055-2026****Project: Terminal Apron & Taxiways H & J Construction****Mead & Hunt Project No. 3098500-222954.01**

Item No.	Item Description	Estimated Quantity	Unit	Unit Price	Total Amount per Item
<u>Bid Schedule 1: General/Civil Construction - Base Bid</u>					
NS-001-4.1	Airfield Safety and Traffic Control	1	LS	\$	\$
NS-002-5.1	Maintenance and Restoration of Haul Roads & Staging Area	1	LS	\$	\$
C-100	Contractor Quality Control Program (CQCP)	1	LS	\$	\$
C-102-5.1	Erosion Control	1	LS	\$	\$
C-105	Mobilization	1	LS	\$	\$
P-101-5.1	Concrete Pavement Removal	1,112	SY	\$	\$
P-101-5.2	Remove Underdrain	134	LF	\$	\$
P-152-4.1	Unclassified Excavation	36,332	CY	\$	\$
P-152-4.2	Compacted Subgrade (12-Inch Depth)	22,339	SY	\$	\$
P-152-4.3	Unclassified Excavation (Over Excavation)	390	CY	\$	\$
P-154-5.1	Subbase Course	8,941	CY	\$	\$
P-154-5.2	Separation Geotextile	24,128	SY	\$	\$
NS-003-6.1	Asphalt Milling Surface Course	467	CY	\$	\$
P-209-5.1	Crushed Aggregate Base Course	3,717	CY	\$	\$
NDDOT 302	NDDOT Aggregate Base Course, Class 5	190	CY	\$	\$
P-501.8.1	11" Concrete Pavement	13,702	SY	\$	\$
P-501.8.2	11" Concrete Pavement, Fully Reinforced	5,085	SY	\$	\$

Item No.	Item Description	Estimated Quantity	Unit	Unit Price	Total Amount per Item
P-501.8.3	8" Concrete Pavement	1,129	SY	\$	\$
P-501.8.4	8" Concrete Pavement, Fully Reinforced	137	SY	\$	\$
P-501.8.5	6" Concrete Pavement	161	SY	\$	\$
P-501.8.6	6" Concrete Pavement, Fully Reinforced	24	SY	\$	\$
NDDOT 550	6" Concrete Pavement	764	SY	\$	\$
NDDOT 550	6" Concrete Pavement, Fully Reinforced	184	SY	\$	\$
P-605-5.1	Joint Sealing Filler	21,893	LF	\$	\$
P-620-5.1	Surface Preparation	1	LS	\$	\$
P-620-5.2	Reflective Media	1	LS	\$	\$
P-620-5.3	Marking Obliteration	1,035	SF	\$	\$
P-620-5.4	Pavement Marking, White	600	SF	\$	\$
P-620-5.5	Pavement Marking, Yellow	1,300	SF	\$	\$
P-620-5.6	Pavement Marking, Red	1,030	SF	\$	\$
P-620-5.7	Pavement Marking, Black	1,840	SF	\$	\$
D-701-5.1	12-inch CMP	34	LF	\$	\$
D-701-5.2	18-inch Class V RCP	620	LF	\$	\$
D-701-5.3	24-inch Class V RCP	205	LF	\$	\$
D-701-5.4	24-inch PVC Pipe	30	LF	\$	\$
D-701-5.5	12-inch CMP End Section	2	EA	\$	\$
D-701-5.6	18-inch RC End Section	6	EA	\$	\$
D-701-5.7	24-inch RC End Section	1	EA	\$	\$
NS-004-5.1	24" Gate Valve with Box and Concrete Collar	1	EA	\$	\$

Item No.	Item Description	Estimated Quantity	Unit	Unit Price	Total Amount per Item
D-705-5.1	4" Perforated Polyethylene Underdrain Pipe - W/Sock, Complete	2,873	LF	\$	\$
D-751-5.1	48-inch Non-aircraft Rated Manhole	1	EA	\$	\$
D-751-5.2	60-inch Aircraft Rated Catch Basin	2	EA	\$	\$
D-751-5.3	60-inch Aircraft Rated Manhole	1	EA	\$	\$
D-751-5.4	60-inch Non-aircraft Rated Manhole	1	EA	\$	\$
D-751-5.5	Trench Drain	1	EA	\$	\$
F-162-5.1	Chain-Link Fence, 10' Galvanized Steel with Barb Wire Top and Mow strip	733	LF	\$	\$
F-162-5.2	Powered 24' Single Cantilevered Slide Gate	1	EA	\$	\$
F-164-5.1	Wildlife Exclusion Fence, 10'	1,430	LF	\$	\$
NS-130-6.1	Gate Operator	1	LS	\$	\$
T-901-5.1	Seeding	11	ACRE	\$	\$
T-905-5.1	Topsoil (Obtained on Site or Removed from Stockpile)	7,710	CY	\$	\$
T-908-5.1	Mulching	11	ACRE	\$	\$
Plan Notes	Erosion Control Blanket	1,275	SY	\$	\$
Plan Notes	Concrete Washout Area	1	EA	\$	\$
Plan Notes	Subgrade Repair Aggregate	263	CY	\$	\$
NDDOT-256	Riprap	14	CY	\$	\$
NDDOT-709	Drainage Fabric (Type D)	21	SY	\$	\$

BID SCHEDULE 1 TOTAL \$

Item No.	Item Description	Estimated Quantity	Unit	Unit Price	Total Amount per Item
<u>Bid Schedule 2: Electrical Construction - Base Bid</u>					
C-105	Mobilization	1	LS	\$	\$
NS-010-4.1	Remove Airfield Electrical Equipment	1	LS	\$	\$
L-108-5.1	No. 8 AWG, 5kV, L-824, Type C Cable, Installed in Trench, Duct Bank or Conduit	3,550	LF	\$	\$
L-108-5.2	No. 6 AWG, Solid, Bare Copper Counterpoise Wire, Installed in Trench, Above the Duct Bank, Conduit or Plowed	2,700	LF	\$	\$
L-108-5.3	No. 8 AWG, 600V Type THWN-2 Cable, Installed in Trench, Duct Bank or Conduit	1,500	LF	\$	\$
L-108-5.4	No. 6 AWG, 600V Type THWN-2 Cable, Installed in Trench, Duct Bank or Conduit	1,500	LF	\$	\$
L-108-5.5	No. 4 AWG, 600V Type THWN-2 Cable, Installed in Trench, Duct Bank or Conduit	1,000	LF	\$	\$
L-108-5.6	No. 10 AWG, 600V Type THWN-2 Cable, Installed in Trench, Duct Bank or Conduit	1,050	LF	\$	\$
L-108-5.7	No. 8 AWG, Insulated Stranded Equipment Ground, Installed in Trench, Duct Bank or Conduit	750	LF	\$	\$
L-108-5.8	No. 6 AWG, Insulated Stranded Equipment Ground, Installed in Trench, Duct Bank or Conduit	550	LF	\$	\$
L-110-5.1	Non-Encased Electrical Conduit, 1W-2"	4,500	LF	\$	\$
L-110-5.2	Concrete Encased Electrical Conduit, 3W-2"	145	LF	\$	\$
L-110-5.3	Non-Encased Electrical Conduit, 1W-1"	350	LF	\$	\$
L-115-5.1	L-867D Base Can Plaza (3 Cans)	2	EA	\$	\$
L-115-5.2	Non-Aircraft H20 Rated Electrical Handhole	2	EA	\$	\$
L-125-5.1	L-861T(L) Taxiway Edge Light on New Base Can	38	EA	\$	\$
L-125-5.2	L-861T(L) Cored-In Taxiway Edge Light on New Base Can	5	EA	\$	\$
L-125-5.3	L-858(L) Guidance Sign, Size 3, 2 Module	2	EA	\$	\$
L-125-5.4	L-858(L) Guidance Sign, Size 3, 3 Module	2	EA	\$	\$
NS-101-4.1	Locate and Protect Existing Equipment	1	LS	\$	\$

Item No.	Item Description	Estimated Quantity	Unit	Unit Price	Total Amount per Item
NS-102-5.1	Temporary Lighting	1	LS	\$ _____	\$ _____
NS-108-5.1	Apron Lighting (LED) Complete - (Fixtures, Pole, and Concrete Foundation)	2	EA	\$ _____	\$ _____
NS-115-5.1	Tier 22 Rated Electrical Handhole 24"x30"	2	EA	\$ _____	\$ _____
NS-131-5.1	Gate Power and Control	1	LS	\$ _____	\$ _____
NS-200-5.1	De-Icing Truck Power Rack and Bollards	1	LS	\$ _____	\$ _____

BID SCHEDULE 2 TOTAL \$ _____

Item No.	Item Description	Estimated Quantity	Unit	Unit Price	Total Amount per Item
<u>Bid Alternate 1A: General/Civil Construction - Bid Alternate #1 (Taxiway J)</u>					
C-100	Contractor Quality Control Program (CQCP)	1	LS	\$ _____	\$ _____
C-105	Mobilization	1	LS	\$ _____	\$ _____
P-101-5.1	Concrete Pavement Removal	1,039	SY	\$ _____	\$ _____
P-101-5.2	Remove Underdrain	133	LF	\$ _____	\$ _____
P-152-4.1	Unclassified Excavation	2,388	CY	\$ _____	\$ _____
P-152-4.2	Compacted Subgrade (12-Inch Depth)	5,427	SY	\$ _____	\$ _____
P-152-4.3	Unclassified Excavation (Over Excavation)	50	CY	\$ _____	\$ _____
P-154-5.1	Subbase Course	2,043	CY	\$ _____	\$ _____
P-154-5.2	Separation Geotextile	4,670	SY	\$ _____	\$ _____
P-209-5.1	Crushed Aggregate Base Course	949	CY	\$ _____	\$ _____
P-501.8.1	11" Concrete Pavement	3,652	SY	\$ _____	\$ _____
P-501.8.2	11" Concrete Pavement, Fully Reinforced	1,274	SY	\$ _____	\$ _____
P-605-5.1	Joint Sealing Filler	7,192	LF	\$ _____	\$ _____

Item No.	Item Description	Estimated Quantity	Unit	Unit Price	Total Amount per Item
P-620-5.1	Surface Preparation	1	LS	\$ _____	\$ _____
P-620-5.2	Reflective Media	1	LS	\$ _____	\$ _____
P-620-5.3	Marking Obliteration	993	SF	\$ _____	\$ _____
P-620-5.5	Pavement Marking, Yellow	600	SF	\$ _____	\$ _____
P-620-5.7	Pavement Marking, Black	1,160	SF	\$ _____	\$ _____
D-705-5.1	4" Perforated Polyethylene Underdrain Pipe - W/Sock, Complete	1,089	LF	\$ _____	\$ _____
T-905-5.1	Topsoil (Obtained on Site or Removed from Stockpile)	773	CY	\$ _____	\$ _____

BID ALTERNATE 1A TOTAL \$ _____

Item No.	Item Description	Estimated Quantity	Unit	Unit Price	Total Amount per Item
<u>Bid Alternate 1B: Electrical Construction - Bid Alternate #1 (Taxiway J)</u>					
C-105	Mobilization	1	LS	\$ _____	\$ _____
NS-010-4.1	Remove Airfield Electrical Equipment	1	LS	\$ _____	\$ _____
L-108-5.1	No. 8 AWG, 5kV, L-824, Type C Cable, Installed in Trench, Duct Bank or C	2,800	LF	\$ _____	\$ _____
L-108-5.2	No. 6 AWG, Solid, Bare Copper Counterpoise Wire, Installed in Trench, Abc	2,030	LF	\$ _____	\$ _____
L-110-5.1	Non-Encased Electrical Conduit, 1W-2"	2,100	LF	\$ _____	\$ _____
L-110-5.2	Concrete Encased Electrical Conduit, 3W-2"	145	LF	\$ _____	\$ _____
L-115-5.1	L-867D Base Can Plaza (3 Cans)	2	EA	\$ _____	\$ _____
L-125-5.1	L-861T(L) Taxiway Edge Light on New Base Can	29	EA	\$ _____	\$ _____

Item No.	Item Description	Estimated Quantity	Unit	Unit Price	Total Amount per Item
L-125-5.3	L-858(L) Guidance Sign, Size 3, 2 Module	1	EA	\$	\$
L-125-5.4	L-858(L) Guidance Sign, Size 3, 3 Module	2	EA	\$	\$
L-125-5.5	L-858(L) Guidance Sign, Size 3, 4 Module	2	EA	\$	\$

BID ALTERNATE 1B TOTAL \$

Item No.	Item Description	Estimated Quantity	Unit	Unit Price	Total Amount per Item
Bid Schedule 3: Combined General/Civil/Electrical Construction - Base Bid					
NS-001-4.1	Airfield Safety and Traffic Control	1	LS	\$	\$
NS-002-5.1	Maintenance and Restoration of Haul Roads & Staging Area	1	LS	\$	\$
C-100	Contractor Quality Control Program (CQCP)	1	LS	\$	\$
C-102-5.1	Erosion Control	1	LS	\$	\$
C-105	Mobilization	1	LS	\$	\$
P-101-5.1	Concrete Pavement Removal	1,112	SY	\$	\$
P-101-5.2	Remove Underdrain	134	LF	\$	\$
P-152-4.1	Unclassified Excavation	36,332	CY	\$	\$
P-152-4.2	Compacted Subgrade (12-Inch Depth)	22,339	SY	\$	\$
P-152-4.3	Unclassified Excavation (Over Excavation)	390	CY	\$	\$
P-154-5.1	Subbase Course	8,941	CY	\$	\$
P-154-5.2	Separation Geotextile	24,128	SY	\$	\$
NS-003-6.1	Asphalt Milling Surface Course	467	CY	\$	\$
P-209-5.1	Crushed Aggregate Base Course	3,717	CY	\$	\$
NDDOT 302	NDDOT Aggregate Base Course, Class 5	190	CY	\$	\$

Item No.	Item Description	Estimated Quantity	Unit	Unit Price	Total Amount per Item
P-501.8.1	11" Concrete Pavement	13,702	SY	\$	\$
P-501.8.2	11" Concrete Pavement, Fully Reinforced	5,085	SY	\$	\$
P-501.8.3	8" Concrete Pavement	1,129	SY	\$	\$
P-501.8.4	8" Concrete Pavement, Fully Reinforced	137	SY	\$	\$
P-501.8.5	6" Concrete Pavement	161	SY	\$	\$
P-501.8.6	6" Concrete Pavement, Fully Reinforced	24	SY	\$	\$
NDDOT 550	6" Concrete Pavement	764	SY	\$	\$
NDDOT 550	6" Concrete Pavement, Fully Reinforced	184	SY	\$	\$
P-605-5.1	Joint Sealing Filler	21,893	LF	\$	\$
P-620-5.1	Surface Preparation	1	LS	\$	\$
P-620-5.2	Reflective Media	1	LS	\$	\$
P-620-5.3	Marking Obliteration	1,035	SF	\$	\$
P-620-5.4	Pavement Marking, White	600	SF	\$	\$
P-620-5.5	Pavement Marking, Yellow	1,300	SF	\$	\$
P-620-5.6	Pavement Marking, Red	1,030	SF	\$	\$
P-620-5.7	Pavement Marking, Black	1,840	SF	\$	\$
D-701-5.1	12-inch CMP	34	LF	\$	\$
D-701-5.2	18-inch Class V RCP	620	LF	\$	\$
D-701-5.3	24-inch Class V RCP	205	LF	\$	\$
D-701-5.4	24-inch PVC Pipe	30	LF	\$	\$
D-701-5.5	12-inch CMP End Section	2	EA	\$	\$
D-701-5.6	18-inch RC End Section	6	EA	\$	\$

Item No.	Item Description	Estimated Quantity	Unit	Unit Price	Total Amount per Item
D-701-5.7	24-inch RC End Section	1	EA	\$	\$
NS-004-5.1	24" Gate Valve with Box and Concrete Collar	1	EA	\$	\$
D-705-5.1	4" Perforated Polyethylene Underdrain Pipe - W/Sock, Complete	2,873	LF	\$	\$
D-751-5.1	48-inch Non-aircraft Rated Manhole	1	EA	\$	\$
D-751-5.2	60-inch Aircraft Rated Catch Basin	2	EA	\$	\$
D-751-5.3	60-inch Aircraft Rated Manhole	1	EA	\$	\$
D-751-5.4	60-inch Non-aircraft Rated Manhole	1	EA	\$	\$
D-751-5.5	Trench Drain	1	EA	\$	\$
F-162-5.1	Chain-Link Fence, 10' Galvanized Steel with Barb Wire Top and Mow strip	733	LF	\$	\$
F-162-5.2	Powered 24' Single Cantilevered Slide Gate	1	EA	\$	\$
F-164-5.1	Wildlife Exclusion Fence, 10'	1,430	LF	\$	\$
NS-130-6.1	Gate Operator	1	LS	\$	\$
T-901-5.1	Seeding	11	ACRE	\$	\$
T-905-5.1	Topsoil (Obtained on Site or Removed from Stockpile)	7,710	CY	\$	\$
T-908-5.1	Mulching	11	ACRE	\$	\$
Plan Notes	Erosion Control Blanket	1,275	SY	\$	\$
Plan Notes	Concrete Washout Area	1	EA	\$	\$
Plan Notes	Subgrade Repair Aggregate	263	CY	\$	\$
NDDOT-256	Riprap	14	CY	\$	\$
NDDOT-709	Drainage Fabric (Type D)	21	SY	\$	\$
NS-010-4.1	Remove Airfield Electrical Equipment	1	LS	\$	\$
L-108-5.1	No. 8 AWG, 5kV, L-824, Type C Cable, Installed in Trench, Duct Bank or C	3,550	LF	\$	\$

Item No.	Item Description	Estimated Quantity	Unit	Unit Price	Total Amount per Item
L-108-5.2	No. 6 AWG, Solid, Bare Copper Counterpoise Wire, Installed in Trench, Abc	2,700	LF	\$	\$
L-108-5.3	No. 8 AWG, 600V Type THWN-2 Cable, Installed in Trench, Duct Bank or C	1,500	LF	\$	\$
L-108-5.4	No. 6 AWG, 600V Type THWN-2 Cable, Installed in Trench, Duct Bank or C	1,500	LF	\$	\$
L-108-5.5	No. 4 AWG, 600V Type THWN-2 Cable, Installed in Trench, Duct Bank or C	1,000	LF	\$	\$
L-108-5.6	No. 10 AWG, 600V Type THWN-2 Cable, Installed in Trench, Duct Bank or	1,050	LF	\$	\$
L-108-5.7	No. 8 AWG, Insulated Stranded Equipment Ground, Installed in Trench, Duc	750	LF	\$	\$
L-108-5.8	No. 6 AWG, Insulated Stranded Equipment Ground, Installed in Trench, Duc	550	LF	\$	\$
L-110-5.1	Non-Encased Electrical Conduit, 1W-2"	4,500	LF	\$	\$
L-110-5.2	Concrete Encased Electrical Conduit, 3W-2"	145	LF	\$	\$
L-110-5.3	Non-Encased Electrical Conduit, 1W-1"	350	LF	\$	\$
L-115-5.1	L-867D Base Can Plaza (3 Cans)	2	EA	\$	\$
L-115-5.2	Non-Aircraft H20 Rated Electrical Handhole	2	EA	\$	\$
L-125-5.1	L-861T(L) Taxiway Edge Light on New Base Can	38	EA	\$	\$
L-125-5.2	L-861T(L) Cored-In Taxiway Edge Light on New Base Can	5	EA	\$	\$
L-125-5.3	L-858(L) Guidance Sign, Size 3, 2 Module	2	EA	\$	\$
L-125-5.4	L-858(L) Guidance Sign, Size 3, 3 Module	2	EA	\$	\$
NS-101-4.1	Locate and Protect Existing Equipment	1	LS	\$	\$

Item No.	Item Description	Estimated Quantity	Unit	Unit Price	Total Amount per Item
NS-102-5.1	Temporary Lighting	1	LS	\$ _____	\$ _____
NS-108-5.1	Apron Lighting (LED) Complete - (Fixtures, Pole, and Concrete Foundation)	2	EA	\$ _____	\$ _____
NS-115-5.1	Tier 22 Rated Electrical Handhole 24"x30"	2	EA	\$ _____	\$ _____
NS-131-5.1	Gate Power and Control	1	LS	\$ _____	\$ _____
NS-200-5.1	De-Icing Truck Power Rack and Bollards	1	LS	\$ _____	\$ _____

BID SCHEDULE 3 TOTAL \$ _____

Item No.	Item Description	Estimated Quantity	Unit	Unit Price	Total Amount per Item
<u>Bid Alternate 1C: Combined General/Civil/Electrical Construction - Bid Alternate #1 (Taxiway J)</u>					
C-100	Contractor Quality Control Program (CQCP)	1	LS	\$ _____	\$ _____
C-105	Mobilization	1	LS	\$ _____	\$ _____
P-101-5.1	Concrete Pavement Removal	1,039	SY	\$ _____	\$ _____
P-101-5.2	Remove Underdrain	133	LF	\$ _____	\$ _____
P-152-4.1	Unclassified Excavation	2,388	CY	\$ _____	\$ _____
P-152-4.2	Compacted Subgrade (12-Inch Depth)	5,427	SY	\$ _____	\$ _____
P-152-4.3	Unclassified Excavation (Over Excavation)	50	CY	\$ _____	\$ _____
P-154-5.1	Subbase Course	2,043	CY	\$ _____	\$ _____
P-154-5.2	Separation Geotextile	4,670	SY	\$ _____	\$ _____
P-209-5.1	Crushed Aggregate Base Course	949	CY	\$ _____	\$ _____
P-501.8.1	11" Concrete Pavement	3,652	SY	\$ _____	\$ _____
P-501.8.2	11" Concrete Pavement, Fully Reinforced	1,274	SY	\$ _____	\$ _____
P-605-5.1	Joint Sealing Filler	7,192	LF	\$ _____	\$ _____

Item No.	Item Description	Estimated Quantity	Unit	Unit Price	Total Amount per Item
P-620-5.1	Surface Preparation	1	LS	\$	\$
P-620-5.2	Reflective Media	1	LS	\$	\$
P-620-5.3	Marking Obliteration	993	SF	\$	\$
P-620-5.5	Pavement Marking, Yellow	600	SF	\$	\$
P-620-5.7	Pavement Marking, Black	1,160	SF	\$	\$
D-705-5.1	4" Perforated Polyethylene Underdrain Pipe - W/Sock, Complete	1,089	LF	\$	\$
T-905-5.1	Topsoil (Obtained on Site or Removed from Stockpile)	773	CY	\$	\$
NS-010-4.1	Remove Airfield Electrical Equipment	1	LS	\$	\$
L-108-5.1	No. 8 AWG, 5kV, L-824, Type C Cable, Installed in Trench, Duct Bank or C	2,800	LF	\$	\$
L-108-5.2	No. 6 AWG, Solid, Bare Copper Counterpoise Wire, Installed in Trench, Abc	2,030	LF	\$	\$
L-110-5.1	Non-Encased Electrical Conduit, 1W-2"	2,100	LF	\$	\$
L-110-5.2	Concrete Encased Electrical Conduit, 3W-2"	145	LF	\$	\$
L-115-5.1	L-867D Base Can Plaza (3 Cans)	2	EA	\$	\$
L-125-5.1	L-861T(L) Taxiway Edge Light on New Base Can	29	EA	\$	\$
L-125-5.3	L-858(L) Guidance Sign, Size 3, 2 Module	1	EA	\$	\$
L-125-5.4	L-858(L) Guidance Sign, Size 3, 3 Module	2	EA	\$	\$
L-125-5.5	L-858(L) Guidance Sign, Size 3, 4 Module	2	EA	\$	\$

BID ALTERNATE 1C TOTAL \$

BID SUMMARY

MULTIPLE PRIME BIDDERS		
BID SCHEDULE 1	GENERAL/CIVIL CONSTRUCTION - BASE BID	\$
BID SCHEDULE 2	ELECTRICAL CONSTRUCTION - BASE BID	\$
BID ALTERNATE 1A	GENERAL/CIVIL CONSTRUCTION - BID ALTERNATE #1 (TAXIWAY J)	\$
BID ALTERNATE 1B	ELECTRICAL CONSTRUCTION - BID ALTERNATE #1 (TAXIWAY J)	\$

SINGLE PRIME BIDDERS		
BID SCHEDULE 3	COMBINED GENERAL/CIVIL/ELECTRICAL CONSTRUCTION - BASE BID	\$
BID ALTERNATE 1C	COMBINED GENERAL/CIVIL/ELECTRICAL CONSTRUCTION - BID ALTERNATE #1 (TAXIWAY J)	\$

Name of Bidder (Typed or Printed) _____

Signature of Bidder (Same as Proposal Form) _____

Title _____

END OF BID SCHEDULE

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THE UNDERSIGNED ALSO DECLARES AND AGREES AS FOLLOWS:

8. The undersigned acknowledges that the FAA does not permit escalation clauses for increases in fuel, asphalt, or other construction costs. Bids shall be adjusted accordingly to account for possible material and labor cost increases over the life of the project.
9. It is understood that the schedule of minimum wage rates, as established by the Secretary of Labor and included in the Specifications, are to govern on this project, and the undersigned certifies that s/he has examined this schedule of wage rates and that the prices bid are based on such established wage rates.
10. The undersigned agree upon written notice of the acceptance of this bid, that within ten (10) days after the award, that s/he will execute the contract in accordance with the bid as accepted and give contract (Performance and Payment) bond on attached forms within ten (10) days after the prescribed forms are presented for signature.
11. The undersigned further agrees that if awarded the contract, s/he will commence the work within ten (10) calendar days after the receipt of a notice to proceed and that s/he will complete the work for all awarded work by completion dates provided in the General Provision Section 80-08. An extension of time may be allowed when extra or additional work is ordered by the engineer. Liquidated damages in the amount specified in the General Provisions, Section 80-08 shall be paid to the Dickinson Municipal Airport Authority for that time which exceeds the completion date provided in General Provision Section 80-08. Further, each phase of work under the project has additional liquidated damage clauses, as outlined in Section 80-08 FAILURE TO COMPLETE ON TIME.
12. As an evidence of good faith in submitting this proposal, the undersigned encloses a bid guarantee in the amount of _____ dollars (\$ _____) which, in case the undersigned refuses or fails to accept an award and to enter into a contract and file the required bonds within the prescribed time, shall be forfeited to the Dickinson Municipal Airport Authority, Dickinson, North Dakota, as liquidated damages.
13. By entering into this contract, the Contractor certifies that neither it (nor s/he) nor any person or firm who has an interest in the Contractor's firm is a person or firm ineligible to be awarded Government contracts by virtue of Item 3(a) of the Davis-Bacon Act or 29 CFR 5.12(a)(1).
14. No part of this contract shall be subcontracted to any person or firm ineligible for award of a Government contract by virtue of Item 3(a) of the Davis-Bacon Act or 29 CFR 5.12(a)(1).
15. The undersigned hereby declares that the only parties interested in this proposal are named herein, that this proposal is made without collusion with any other person, or corporation. That no member of the council, officer or agent of North Dakota, is directly or indirectly financially interested in this bid.

16. The undersigned acknowledges receipt of the following Addendums:

Addendum No. _____	Date Received _____
Addendum No. _____	Date Received _____
Addendum No. _____	Date Received _____
Addendum No. _____	Date Received _____
Addendum No. _____	Date Received _____

NON-COLLUSION DECLARATION

I, by signing the proposal, hereby declare, under penalty of perjury under the laws of the United States that the following statements are true and correct:

1. That the undersigned person(s), firm, association or corporation has (have) not, either directly or indirectly, entered into any agreement, participated in any collusion, or otherwise taken any action in restraint of free competitive bidding in connection with the project for which this proposal is submitted.

2. That by signing the signature page of this proposal, I am deemed to have signed and to have agreed to the provisions of this declaration.

DIVISION 3

CONTRACT AGREEMENT

Dickinson Municipal Airport Authority - Dickinson Theodore Roosevelt Regional Airport

Dickinson, North Dakota

AIP PROJECT NO. 3-38-0013-055-2026

THIS AGREEMENT, made and entered into this _____ day of _____, 20____, by and between Dickinson Municipal Airport Authority, Party of the First Part, hereinafter referred to as the “Owner”, and _____, Party of the Second Part, hereinafter referred to as the “Contractor,” for the construction of airport improvement including _____ and other incidental work at the Dickinson Theodore Roosevelt Regional Airport.

WITNESSETH:

WORK TO BE PERFORMED. The Contractor agrees to do all the work and furnish all necessary labor, materials, tools and equipment for the completion of the Terminal Apron & Taxiways H & J Construction project in accordance with the bid made by the Contractor on the 11th day of June 2026, all in full compliance with the Contract Documents referred to herein, and guarantees all materials and workmanship for one year after acceptance of the project.

CONTRACT DOCUMENTS. It is hereby further agreed that any references herein to the “Contract” shall include “Contract Documents” as the same as defined in Paragraph 10-13, Section 10 of the General Provisions and consisting of the Advertisement for Bid, Instruction to Bidders, all Proposal Forms contained under Division 2, all issued Addenda, all Contract Forms contained under Division 3, FAA General Provisions, Special Provisions, Construction Safety & Phasing Plan (CSPP), Wage Rates, Plans, Technical Specifications, attached appendices and all documents incorporated by reference. Said “Contract Documents” are made a part of the Contract as if set out at length herein. Said Contract Agreement is limited to the items in the proposal as signed by the “Contractor” and included in the “Contract Documents.”

The Contractor agrees to perform all the work describe in the Contract Documents for the unit prices and lump sums as submitted in the Bid, taking into consideration additions to or deductions from the Total Bid by reason of alterations or modifications of the original quantities or by reason of “Extra Work” authorized under this Agreement in accordance with the provisions of the Contract Documents.

Each contract shall be executed in five original copies and there shall be executed originals of the Contractor's Performance Bond and Payment Bond in equal number to the executed originals of the contract. Two copies of such executed documents will be retained by the Dickinson Municipal Airport Authority, one copy shall be delivered to the FAA, and two copies will be delivered to the Contractor. The cost of executing the Contract, bonds and insurance, including all notary fees and incidental expenses are to be paid by the Contractor to whom the contract is awarded.

PAYMENT. It is hereby further agreed that in consideration of the faithful performance of the work by the Contractor, the Owner shall pay the Contractor the compensation due him/her by reason of said faithful performance of the work, at stated intervals and in the amount certified by the Engineer, in accordance with the provisions of this Contract.

It is hereby further agreed that, at the completion of the work and its acceptance by the Owner, all sums due the Contractor by reason of his faithful performance of the work, taking into consideration additions to or deductions from the Contract price by reason of alterations or modifications of the original Contract or by reason of “Extra Work” authorized under this Contract, will be paid the Contractor by the Owner after said completion and acceptance.

TIME OF PERFORMANCE – LIQUIDATED DAMAGES. The Contractor shall commence work under this Contract within ten (10) calendar days after receiving notification to proceed from the Dickinson Municipal Airport Authority. The Contractor agrees that the work under this Contract shall be completed, accepted, and ready for final payment by August 27, 2027 after notification to begin work. If the Contractor fails to complete the Project within the time hereinbefore mentioned, or in the extended time agreed upon, liquidated damages shall be paid to or withheld by the Owner at the rate specified in the General Provisions, Section 80-08 until the Project is completed. Further, each schedule of work and phase under the project has additional liquidated damage clauses, as outlined in Section 80-08 Failure to Complete on Time.

It has been agreed that the damages arising from a delay in completion would be difficult to ascertain with any degree of accuracy, even after the Project is completed. It has also been agreed that the amount of liquidated damages specified herein is a reasonable forecast of just compensation for the harm that will be caused by a delay in completion of the Project. Any such sum which the Contractor may be obligated to pay under the terms of this paragraph is paid as liquidated damages, and not as a penalty.

Further, each phase of work under the project has additional liquidated damage clauses, as outlined in Section 80-08 FAILURE TO COMPLETE ON TIME.

The total estimated cost for AIP project #3-38-0013-055-2026 thereof to be

(\$_____).

IN WITNESS WHEREOF, the Party of the First Part and the Party of the Second Part, respectively, have caused this Agreement to be duly executed in day and year first herein written.

CONTRACTOR, Party of the Second Part

OWNER, Party of the First Part

By: _____

By: _____

(Office or Position of Signer)

(Office or Position of Signer)

(SEAL)

(SEAL)

ATTEST: _____

ATTEST: _____

(Office or Position of Signer)

(Office or Position of Signer)

Section 80 Execution and Progress

80-01 Subletting of contract. The Owner will not recognize any subcontractor on the work. The Contractor shall at all times when work is in progress be represented either in person, by a qualified superintendent, or by other designated, qualified representative who is duly authorized to receive and execute orders of the Resident Project Representative (RPR).

The Contractor shall perform, with his organization, an amount of work equal to at least 25 percent of the total contract cost.

Should the Contractor elect to assign their contract, said assignment shall be concurred in by the surety, shall be presented for the consideration and approval of the Owner, and shall be consummated only on the written approval of the Owner.

The Contractor shall provide copies of all subcontracts to the RPR 14 days prior to being utilized on the project. As a minimum, the information shall include the following:

- Subcontractor's legal company name.
- Subcontractor's legal company address, including County name.
- Principal contact person's name, telephone and fax number.
- Complete narrative description, and dollar value of the work to be performed by the subcontractor.
- Copies of required insurance certificates in accordance with the specifications.
- Minority/ non-minority status.

80-02 Notice to proceed (NTP). The Owners notice to proceed will state the date on which contract time commences. The Contractor is expected to commence project operations within 10 days of the NTP date. The Contractor shall notify the RPR at least 24 hours in advance of the time contract operations begins. The Contractor shall not commence any actual operations prior to the date on which the notice to proceed is issued by the Owner.

80-03 Execution and progress. Unless otherwise specified, the Contractor shall submit their coordinated construction schedule showing all work activities for the RPR's review and acceptance at least 10 days prior to the start of work. The Contractor's progress schedule, once accepted by the RPR, will represent the Contractor's baseline plan to accomplish the project in accordance with the terms and conditions of the Contract. The RPR will compare actual Contractor progress against the baseline schedule to determine that status of the Contractor's performance. The Contractor shall provide sufficient materials, equipment, and labor to guarantee the completion of the project in accordance with the plans and specifications within the time set forth in the proposal.

If the Contractor falls significantly behind the submitted schedule, the Contractor shall, upon the RPR's request, submit a revised schedule for completion of the work within the contract time and modify their operations to provide such additional materials, equipment, and labor necessary to meet the revised schedule. Should the execution of the work be discontinued for any reason, the Contractor shall notify the RPR at least 24 hours in advance of resuming operations.

The Contractor shall not commence any actual construction prior to the date on which the NTP is issued by the Owner.

The project schedule shall be prepared as a network diagram in Critical Path Method (CPM), Program Evaluation and Review Technique (PERT), or other format, or as otherwise specified. It shall include information on the sequence of work activities, milestone dates, and activity duration. The schedule shall

show all work items identified in the project proposal for each work area and shall include the project start date and end date.

The Contractor shall maintain the work schedule and provide an update and analysis of the progress schedule on a twice monthly basis, or as otherwise specified in the contract. Submission of the work schedule shall not relieve the Contractor of overall responsibility for scheduling, sequencing, and coordinating all work to comply with the requirements of the contract.

80-04 Limitation of operations. The Contractor shall control their operations and the operations of their subcontractors and all suppliers to provide for the free and unobstructed movement of aircraft in the air operations areas (AOA) of the airport.

When the work requires the Contractor to conduct their operations within an AOA of the airport, the work shall be coordinated with airport operations (through the RPR) at least 48 hours prior to commencement of such work. The Contractor shall not close an AOA until so authorized by the RPR and until the necessary temporary marking, signage and associated lighting is in place as provided in Section 70, paragraph 70-08, *Construction Safety and Phasing Plan (CSPP)*.

When the contract work requires the Contractor to work within an AOA of the airport on an intermittent basis (intermittent opening and closing of the AOA), the Contractor shall maintain constant communications as specified; immediately obey all instructions to vacate the AOA; and immediately obey all instructions to resume work in such AOA. Failure to maintain the specified communications or to obey instructions shall be cause for suspension of the Contractor's operations in the AOA until satisfactory conditions are provided. The areas of the AOA identified in the Construction Safety Phasing Plan (CSPP) and as listed below, cannot be closed to operating aircraft to permit the Contractor's operations on a continuous basis and will therefore be closed to aircraft operations intermittently as shown on the plans.

The Contractor shall be required to conform to safety standards contained in AC 150/5370-2, Operational Safety on Airports During Construction and the approved CSPP.

80-04.1 Operational safety on airport during construction. All Contractors' operations shall be conducted in accordance with the approved project Construction Safety and Phasing Plan (CSPP) and the Safety Plan Compliance Document (SPCD) and the provisions set forth within the current version of AC 150/5370-2, Operational Safety on Airports During Construction. The CSPP included within the contract documents conveys minimum requirements for operational safety on the airport during construction activities. The Contractor shall prepare and submit a SPCD that details how it proposes to comply with the requirements presented within the CSPP.

The Contractor shall implement all necessary safety plan measures prior to commencement of any work activity. The Contractor shall conduct routine checks to assure compliance with the safety plan measures.

The Contractor is responsible to the Owner for the conduct of all subcontractors it employs on the project. The Contractor shall assure that all subcontractors are made aware of the requirements of the CSPP and SPCD and that they implement and maintain all necessary measures.

No deviation or modifications may be made to the approved CSPP and SPCD unless approved in writing by the Owner. The necessary coordination actions to review Contractor proposed modifications to an approved CSPP or approved SPCD can require a significant amount of time.

80-05 Character of workers, methods, and equipment. The Contractor shall, at all times, employ sufficient labor and equipment for prosecuting the work to full completion in the manner and time required by the contract, plans, and specifications.

All workers shall have sufficient skill and experience to perform properly the work assigned to them. Workers engaged in special work or skilled work shall have sufficient experience in such work and in the operation of the equipment required to perform the work satisfactorily.

Any person employed by the Contractor or by any subcontractor who violates any operational regulations or operational safety requirements and, in the opinion of the RPR, does not perform his work in a proper and skillful manner or is intemperate or disorderly shall, at the written request of the RPR, be removed immediately by the Contractor or subcontractor employing such person, and shall not be employed again in any portion of the work without approval of the RPR.

Should the Contractor fail to remove such person or persons, or fail to furnish suitable and sufficient personnel for the proper execution of the work, the RPR may suspend the work by written notice until compliance with such orders.

All equipment that is proposed to be used on the work shall be of sufficient size and in such mechanical condition as to meet requirements of the work and to produce a satisfactory quality of work. Equipment used on any portion of the work shall not cause injury to previously completed work, adjacent property, or existing airport facilities due to its use.

When the methods and equipment to be used by the Contractor in accomplishing the work are not prescribed in the contract, the Contractor is free to use any methods or equipment that will accomplish the work in conformity with the requirements of the contract, plans, and specifications.

When the contract specifies the use of certain methods and equipment, such methods and equipment shall be used unless otherwise authorized by the RPR. If the Contractor desires to use a method or type of equipment other than specified in the contract, the Contractor may request authority from the RPR to do so. The request shall be in writing and shall include a full description of the methods and equipment proposed and of the reasons for desiring to make the change. If approval is given, it will be on the condition that the Contractor will be fully responsible for producing work in conformity with contract requirements. If, after trial use of the substituted methods or equipment, the RPR determines that the work produced does not meet contract requirements, the Contractor shall discontinue the use of the substitute method or equipment and shall complete the remaining work with the specified methods and equipment. The Contractor shall remove any deficient work and replace it with work of specified quality, or take such other corrective action as the RPR may direct. No change will be made in basis of payment for the contract items involved nor in contract time as a result of authorizing a change in methods or equipment under this paragraph.

80-06 Temporary suspension of the work. The Owner shall have the authority to suspend the work wholly, or in part, for such period or periods the Owner may deem necessary, due to unsuitable weather, or other conditions considered unfavorable for the execution of the work, or for such time necessary due to the failure on the part of the Contractor to carry out orders given or perform any or all provisions of the contract.

In the event that the Contractor is ordered by the Owner, in writing, to suspend work for some unforeseen cause not otherwise provided for in the contract and over which the Contractor has no control, the Contractor may be reimbursed for actual money expended on the work during the period of shutdown. No allowance will be made for anticipated profits. The period of shutdown shall be computed from the effective date of the written order to suspend work to the effective date of the written order to resume the work. Claims for such compensation shall be filed with the RPR within the time period stated in the RPR's order to resume work. The Contractor shall submit with their own claim information substantiating the amount shown on the claim. The RPR will forward the Contractor's claim to the Owner for consideration in accordance with local laws or ordinances. No provision of this article shall be construed as entitling the Contractor to compensation for delays due to inclement weather or for any other delay provided for in the contract, plans, or specifications.

If it becomes necessary to suspend work for an indefinite period, the Contractor shall store all materials in such manner that they will not become an obstruction nor become damaged in any way. The Contractor shall take every precaution to prevent damage or deterioration of the work performed and provide for

normal drainage of the work. The Contractor shall erect temporary structures where necessary to provide for traffic on, to, or from the airport.

80-07 Determination and extension of contract time. The completion date shall be stated in the proposal and contract and shall be known as the Contract Time.

If the contract time requires extension for reasons beyond the Contractor’s control, it shall be adjusted as follows:

Contract time based on specific completion date. When the contract time is a specified completion date, it shall be the date on which all contract work shall be substantially complete.

If the Contractor finds it impossible for reasons beyond their own control to complete the work within the contract time as specified, or as extended in accordance with the provisions of this paragraph, the Contractor may, at any time prior to the expiration of the contract time as extended, make a written request to the Owner for an extension of time setting forth the reasons which the Contractor believes will justify the granting of their own request. Requests for extension of time, caused by inclement weather, shall be supported with National Weather Bureau data showing the actual amount of inclement weather exceeded what could normally be expected during the contract period. The Contractor’s plea that insufficient time was specified is not a valid reason for extension of time. If the supporting documentation justify the work was delayed because of conditions beyond the control and without the fault of the Contractor, the Owner may extend the time for completion by a change order that adjusts the contract time or completion date. The extended time for completion shall then be in full force and effect, the same as though it were the original time for completion.

80-08 Failure to complete on time. For each calendar day or working day, as specified in the contract, that any work remains uncompleted after the contract time (including all extensions and adjustments as provided in paragraph 80-07, *Determination and Extension of Contract Time*) the sum specified in the contract and proposal as liquidated damages (LD) will be deducted from any money due or to become due the Contractor or their own surety. Such deducted sums shall not be deducted as a penalty but shall be considered as liquidation of a reasonable portion of damages including but not limited to additional engineering services that will be incurred by the Owner should the Contractor fail to complete the work in the time provided in their contract.

Schedule & Phase	Liquidated Damages Cost	Completion Date
Base Bid & Alternate 1 Phase 1	See Table Below	August 27, 2027
Base Bid & Alternate 1 Phase 2	See Table Below	August 27, 2027

Original Contract		Amount of Liquidated Damages per Day		
Over	To and Including	Prerequisites to Substantial Completion		Prerequisites to Final Acceptance
		Calendar Day	Working Day	Calendar Day
\$0	\$100,000	\$350	\$400	\$70
\$100,000	\$250,000	\$700	\$1,000	\$140
\$250,000	\$500,000	\$900	\$1,400	\$180
\$500,000	\$1,000,000	\$1,300	\$1,800	\$260
\$1,000,000	\$3,000,000	\$1,600	\$2,300	\$320
\$3,000,000	\$5,000,000	\$2,300	\$3,000	\$460
\$5,000,000	\$8,000,000	\$2,800	\$3,600	\$560
\$8,000,000	\$12,000,000	\$3,300	\$4,100	\$660
\$12,000,000	\$18,000,000	\$4,000	\$4,600	\$800
\$18,000,000	Up	\$5,000	\$5,600	\$1,000

Permitting the Contractor to continue and finish the work or any part of it after the time fixed for its completion, or after the date to which the time for completion may have been extended, will in no way operate as a waiver on the part of the Owner of any of its rights under the contract.

80-09 Default and termination of contract. The Contractor shall be considered in default of their contract and such default will be considered as cause for the Owner to terminate the contract for any of the following reasons, if the Contractor:

- a. Fails to begin the work under the contract within the time specified in the Notice to Proceed, or
- b. Fails to perform the work or fails to provide sufficient workers, equipment and/or materials to assure completion of work in accordance with the terms of the contract, or
- c. Performs the work unsuitably or neglects or refuses to remove materials or to perform anew such work as may be rejected as unacceptable and unsuitable, or
- d. Discontinues the execution of the work, or
- e. Fails to resume work which has been discontinued within a reasonable time after notice to do so, or
- f. Becomes insolvent or is declared bankrupt, or commits any act of bankruptcy or insolvency, or
- g. Allows any final judgment to stand against the Contractor unsatisfied for a period of 10 days, or
- h. Makes an assignment for the benefit of creditors, or
- i. For any other cause whatsoever, fails to carry on the work in an acceptable manner.

Should the Owner consider the Contractor in default of the contract for any reason above, the Owner shall immediately give written notice to the Contractor and the Contractor’s surety as to the reasons for considering the Contractor in default and the Owner’s intentions to terminate the contract.

If the Contractor or surety, within a period of 10 days after such notice, does not proceed in accordance therewith, then the Owner will, upon written notification from the RPR of the facts of such delay, neglect, or default and the Contractor’s failure to comply with such notice, have full power and authority without

violating the contract, to take the execution of the work out of the hands of the Contractor. The Owner may appropriate or use any or all materials and equipment that have been mobilized for use in the work and are acceptable and may enter into an agreement for the completion of said contract according to the terms and provisions thereof, or use such other methods as in the opinion of the RPR will be required for the completion of said contract in an acceptable manner.

All costs and charges incurred by the Owner, together with the cost of completing the work under contract, will be deducted from any monies due or which may become due the Contractor. If such expense exceeds the sum which would have been payable under the contract, then the Contractor and the surety shall be liable and shall pay to the Owner the amount of such excess.

80-10 Termination for national emergencies. The Owner shall terminate the contract or portion thereof by written notice when the Contractor is prevented from proceeding with the construction contract as a direct result of an Executive Order of the President with respect to the execution of war or in the interest of national defense.

When the contract, or any portion thereof, is terminated before completion of all items of work in the contract, payment will be made for the actual number of units or items of work completed at the contract price or as mutually agreed for items of work partially completed or not started. No claims or loss of anticipated profits shall be considered.

Reimbursement for organization of the work, and other overhead expenses, (when not otherwise included in the contract) and moving equipment and materials to and from the job will be considered, the intent being that an equitable settlement will be made with the Contractor.

Acceptable materials, obtained or ordered by the Contractor for the work and that are not incorporated in the work shall, at the option of the Contractor, be purchased from the Contractor at actual cost as shown by receipted bills and actual cost records at such points of delivery as may be designated by the RPR.

Termination of the contract or a portion thereof shall neither relieve the Contractor of their responsibilities for the completed work nor shall it relieve their surety of its obligation for and concerning any just claim arising out of the work performed.

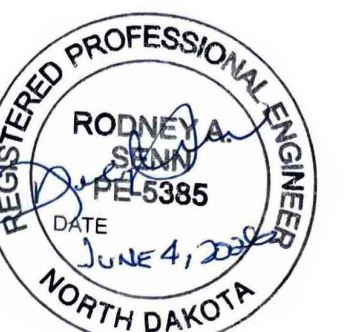
80-11 Work area, storage area and sequence of operations. The Contractor shall obtain approval from the RPR prior to beginning any work in all areas of the airport. No operating runway, taxiway, or air operations area (AOA) shall be crossed, entered, or obstructed while it is operational. The Contractor shall plan and coordinate work in accordance with the approved CSPP and SPCD.

END OF SECTION 80

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**THEODORE ROOSEVELT REGIONAL AIRPORT
TERMINAL APRON & TAXIWAYS H & J
CONSTRUCTION**
11120 42ND ST SW
DICKINSON, NORTH DAKOTA

ISSUED
1 06/03/26 ADDENDUM 1



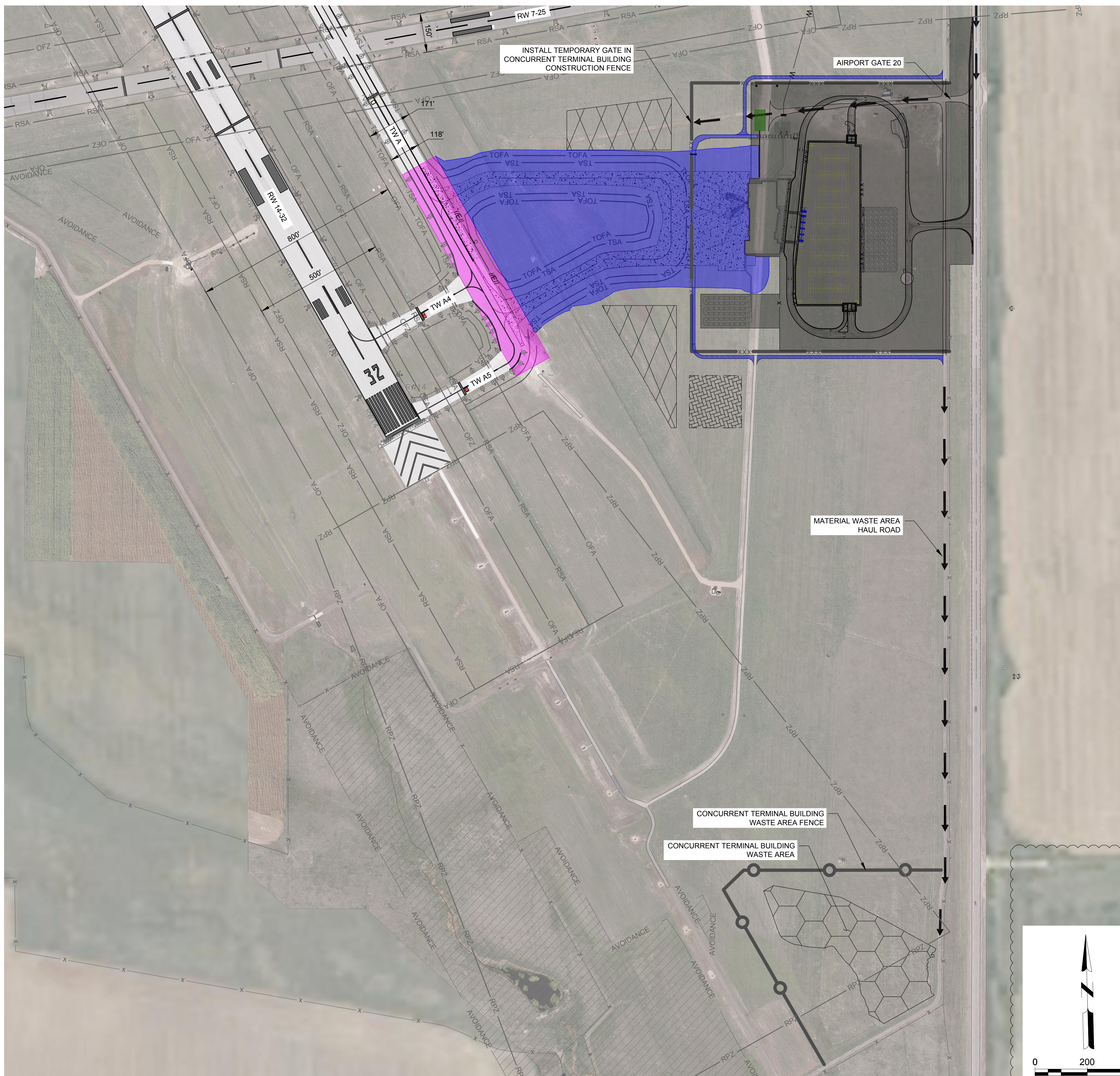
ISSUED FOR BID

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MSH NO: 3098500-222954.01
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DRAWN BY: JAK
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SHEET CONTENTS
CONSTRUCTION
SAFETY & PHASING
PLAN

SHEET NO.

G-081R

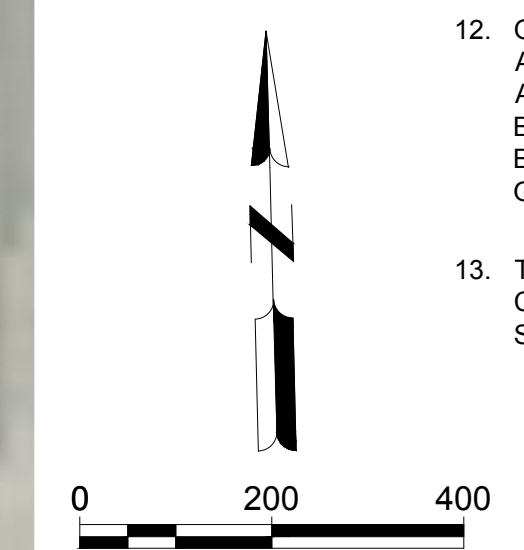


LEGEND:

- HAUL ROUTE
- RSA RUNWAY SAFETY AREA
- OFZ RUNWAY OBJECT FREE ZONE
- ROFA RUNWAY OBJECT FREE AREA
- TOFA TAXIWAY OBJECT FREE AREA
- TSA TAXIWAY SAFETY AREA
- EXISTING AIRPORT PERIMETER FENCE
- CONCURRENT TERMINAL BUILDING CONSTRUCTION FENCE
- CONTRACTOR STORAGE/STAGING AREA
- TOPSOIL STOCKPILE AREA
- PHASE 1 WORK LIMITS
- PHASE 2 WORK LIMITS
- MATERIAL WASTE AREA
- CONCURRENT CONSTRUCTION
- AVOIDANCE AVOIDANCE AREA

CONSTRUCTION SAFETY AND PHASING NOTES

1. SEE CONSTRUCTION SAFETY AND PHASING PLAN NARRATIVE FOR ADDITIONAL REQUIREMENTS.
2. THE CONTRACTOR SHALL COMPLY WITH ALL REQUIREMENTS SET FORTH IN FAA AC 150/5370-2G.
3. AT ALL TIMES DURING CONSTRUCTION THE AIRPORT PERIMETER FENCE MUST BE SECURE.
4. THE CONTRACTOR SHALL PROVIDE, INSTALL AND MAINTAIN ALL TRAFFIC CONTROL DEVICES. ALL TRAFFIC CONTROL ITEMS SHALL CONFORM TO THE UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION.
5. SEE AIRSPACE POINT TABLES FOR THE EXPECTED MAXIMUM HEIGHT OF CONSTRUCTION EQUIPMENT ABOVE GROUND LEVEL (AGL). THE CONTRACTOR SHALL VERIFY ANY EQUIPMENT HEIGHT MORE THAN HEIGHT SHOWN AND SUBMIT REVISIONS FOR REVIEW AND INCORPORATION INTO THE SAFETY PLAN.
6. ANY AND ALL DAMAGE TO THE EXISTING PAVEMENTS, TURF AND ROADS SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER. THE CONTRACTOR SHALL TAKE THIS INTO ACCOUNT WHEN CHOOSING THE TYPES OF CONSTRUCTION EQUIPMENT AND EQUIPMENT LOADING.
7. ALL VEHICLES AND EQUIPMENT MUST BE EQUIPPED WITH AN OMNIDIRECTIONAL AMBER FLASHING LIGHT DURING THE NIGHT OR AVIATION CONSTRUCTION FLAGS PER DETAILS FOR DAYTIME OPERATIONS.
8. CONTRACTOR SHALL SUPPLY, MAINTAIN, AND REMOVE TEMPORARY FENCE AS SHOWN IN THE PLANS. CONCURRENT APRON AND PARKING LOT/ACCESS ROAD PROJECTS MAY REQUIRE ADJUSTMENT OF TEMPORARY FENCE TO ACCOMMODATE CONSTRUCTION. CONTRACTOR SHALL ADJUST TEMPORARY FENCE AT THE DIRECTION OF THE ENGINEER TO FACILITATE CONCURRENT CONSTRUCTION.
9. CONTRACTOR SHALL COORDINATE WITH AIRPORT AND ENGINEER FOR EXACT FENCE PLACEMENT.
10. IT IS ANTICIPATED THAT CONCURRENT CONSTRUCTION PROJECTS WILL OCCUR FOR CONSTRUCTING THE TERMINAL BUILDING, PARKING LOTS, AND ACCESS ROADS. THIS CONCURRENT CONSTRUCTION WILL BE ADJACENT TO THE FACILITIES CONSTRUCTED WITHIN THIS PROJECT. CONTRACTOR SHALL COORDINATE WITH OWNER, ENGINEER AND ADJACENT CONTRACTORS AS NECESSARY. THE OWNER AND ENGINEER SHALL BE INCLUDED IN ALL COORDINATION BETWEEN CONTRACTORS.
11. THE CONCURRENT TERMINAL BUILDING CONTRACTOR HAS INSTALLED A TEMPORARY 10' CHAINLINK CONSTRUCTION FENCE AROUND THEIR WORK AREA, SHOWN ON THIS SHEET AS THE CONCURRENT TERMINAL BUILDING CONSTRUCTION FENCE. THE CONTRACTOR SHALL ACCESS SITE THROUGH AIRPORT GATE 20 AND A TEMPORARY GATE INSTALLED IN TEMPORARY CONSTRUCTION FENCE INSTALLED BY THE CONTRACTOR. WHEN THE TEMPORARY GATE IS TO REMAIN OPEN A GATE GUARD MUST BE PRESENT AT ALL TIMES. WHEN THE GATE IS NOT IN USE OR THE CONTRACTOR IS NOT ON SITE THE GATE SHALL BE LOCKED.
12. ONCE THE CONCURRENT TERMINAL BUILDING CONTRACTOR CONSTRUCTION FENCE IS REMOVED AND THE PROPOSED PERMANENT SECURITY AND WILDLIFE FENCES ARE NOT YET IN PLACE, SITE ACCESS SHALL BE THROUGH AIRPORT GATE 20. WHEN THE GATE IS IN USE A GATE GUARD SHALL BE PRESENT AT ALL TIMES. THE CONTRACTOR SHALL COORDINATE WITH THE TERMINAL BUILDING CONTRACTOR ON WHO WILL BE RESPONSIBLE ON PROVIDING A GATE GUARD ON A GIVEN DAY.
13. TERMINAL BUILDING CONTRACTOR IS SCHEDULED TO BEGIN CONSTRUCTION IN APRIL 2026 WITH COMPLETION IN NOVEMBER 2027. PRELIMINARY SCHEDULES INDICATE THE TERMINAL BUILDING STRUCTURE WILL BE ERECT AND ENCLOSED BY DECEMBER 2026.



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THEODORE ROOSEVELT REGIONAL AIRPORT TERMINAL APRON & TAXIWAYS H & J CONSTRUCTION 11120 42ND ST SW DICKINSON, NORTH DAKOTA

ISSUED FOR BID
1 06/03/26 ADDENDUM 1



ISSUED FOR BID

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MSH NO: 3098500-222954.01
DATE: MAY 19, 2026
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DRAWN BY: JAK
CHECKED BY: RDS
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SHEET CONTENTS
CONSTRUCTION
SAFETY & PHASING
PLAN - PHASE 1

SHEET NO.

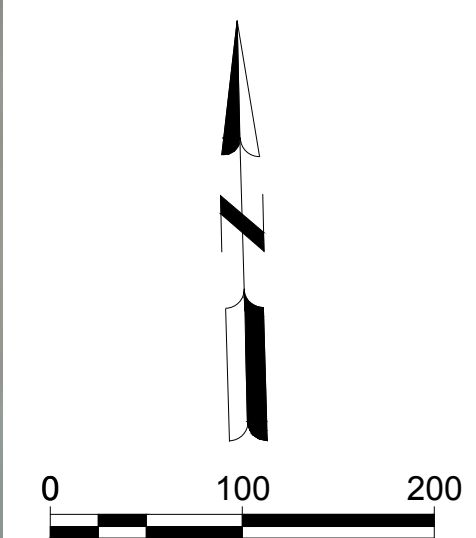
G-082

LEGEND:

- HAUL ROUTE
- RSA RUNWAY SAFETY AREA
- OFZ RUNWAY OBJECT FREE ZONE
- ROFA RUNWAY OBJECT FREE AREA
- TOFA TAXIWAY OBJECT FREE AREA
- TSA TAXIWAY SAFETY AREA
- EXISTING AIRPORT PERIMETER FENCE
- CONCURRENT TERMINAL BUILDING CONSTRUCTION FENCE
- TEMPORARY CONSTRUCTION FENCE (PHASE 1 ONLY)
- TEMPORARY CONSTRUCTION FENCE (PHASE 1 AND 2)
- LOW PROFILE BARRICADES
- CONTRACTOR STORAGE/STAGING AREA
- TOPSOIL STOCKPILE AREA
- PHASE 1 WORK LIMITS
- CONCURRENT CONSTRUCTION

PHASE 1 NOTES

1. PHASE 1 WORK INCLUDES REMOVAL OF EXISTING PAVEMENT, REMOVAL OF EXISTING ELECTRICAL, AND AGGREGATE SUBBASE AND BASE COURSE, CONCRETE PAVEMENT, UNDERDRAIN, STORM DRAIN, ELECTRICAL INSTALLATION, AND ASSOCIATED EARTHWORK AND TOPSOIL PLACEMENT WITHIN WORK AREAS SHOWN.
2. PHASE 1 SHALL BE COMPLETED BY AUGUST 27, 2027. THE CONTRACTOR MAY START CONSTRUCTION IN THE FALL OF 2026 ONCE THE NOTICE TO PROCEED HAS BEEN GIVEN BY THE OWNER.
3. TAXIWAY A SHALL BE CLOSED FOR A MAXIMUM OF 30 CALENDAR DAYS.
4. THIS SECTION OF TEMPORARY CONSTRUCTION FENCE SHALL BE REMOVED AT THE COMPLETION OF PHASE 1.
5. THIS SECTION OF TEMPORARY CONSTRUCTION FENCE SHALL REMAIN IN PLACE FOR PHASE 2 CONSTRUCTION.
















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THEODORE ROOSEVELT REGIONAL AIRPORT TERMINAL APRON & TAXIWAYS H & J CONSTRUCTION 11120 42ND ST SW DICKINSON, NORTH DAKOTA

LEGEND:

-  HAUL ROUTE
-  RUNWAY SAFETY AREA
-  RUNWAY OBJECT FREE ZONE
-  RUNWAY OBJECT FREE AREA
-  TAXIWAY OBJECT FREE AREA
-  TAXIWAY SAFETY AREA
-  EXISTING AIRPORT PERIMETER FENCE
-  CONCURRENT TERMINAL BUILDING CONSTRUCTION FENCE
-  TEMPORARY CONSTRUCTION FENCE
-  CONTRACTOR STORAGE/STAGING AREA
-  TOPSOIL STOCKPILE AREA
-  PHASE 2 WORK LIMITS
-  CONCURRENT CONSTRUCTION

PHASE 2 NOTES

1. PHASE 2 WORK INCLUDES AGGREGATE SUBBASE AND BASE COURSE, CONCRETE PAVEMENT, UNDERDRAIN, STORM DRAIN, ELECTRICAL INSTALLATION, AND ASSOCIATED EARTHWORK AND TOPSOIL PLACEMENT WITHIN WORK AREAS SHOWN.
2. THE CONTRACTOR TO COORDINATE WITH ENGINEER, THE AIRPORT, AND CONCURRENT TERMINAL BUILDING CONTRACTOR FOR REMOVAL/RELOCATION OF THE CONCURRENT TERMINAL BUILDING CONSTRUCTION FENCE FOR APRON CONSTRUCTION. AIRPORT PERIMETER FENCE TO BE MAINTAINED AT ALL TIMES. GATE GUARD AT EXISTING GATE 20 TO BE USED IF AIRPORT PERIMETER FENCE IS NOT CONTINUOUS.
3. PHASE 2 CAN BE WORKED ON CONCURRENTLY WITH PHASE 1.
4. PHASE 2 WORK WITHIN THE CONCURRENT TERMINAL BUILDING CONSTRUCTION FENCE CANNOT START UNTIL 2027.
5. PHASE 2 SHALL BE COMPLETED BY AUGUST 27, 2027.

ISSUED FOR BID
1 06/03/26 ADDENDUM 1



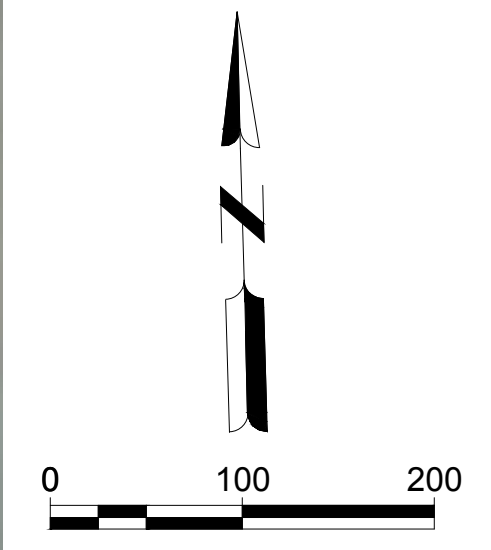
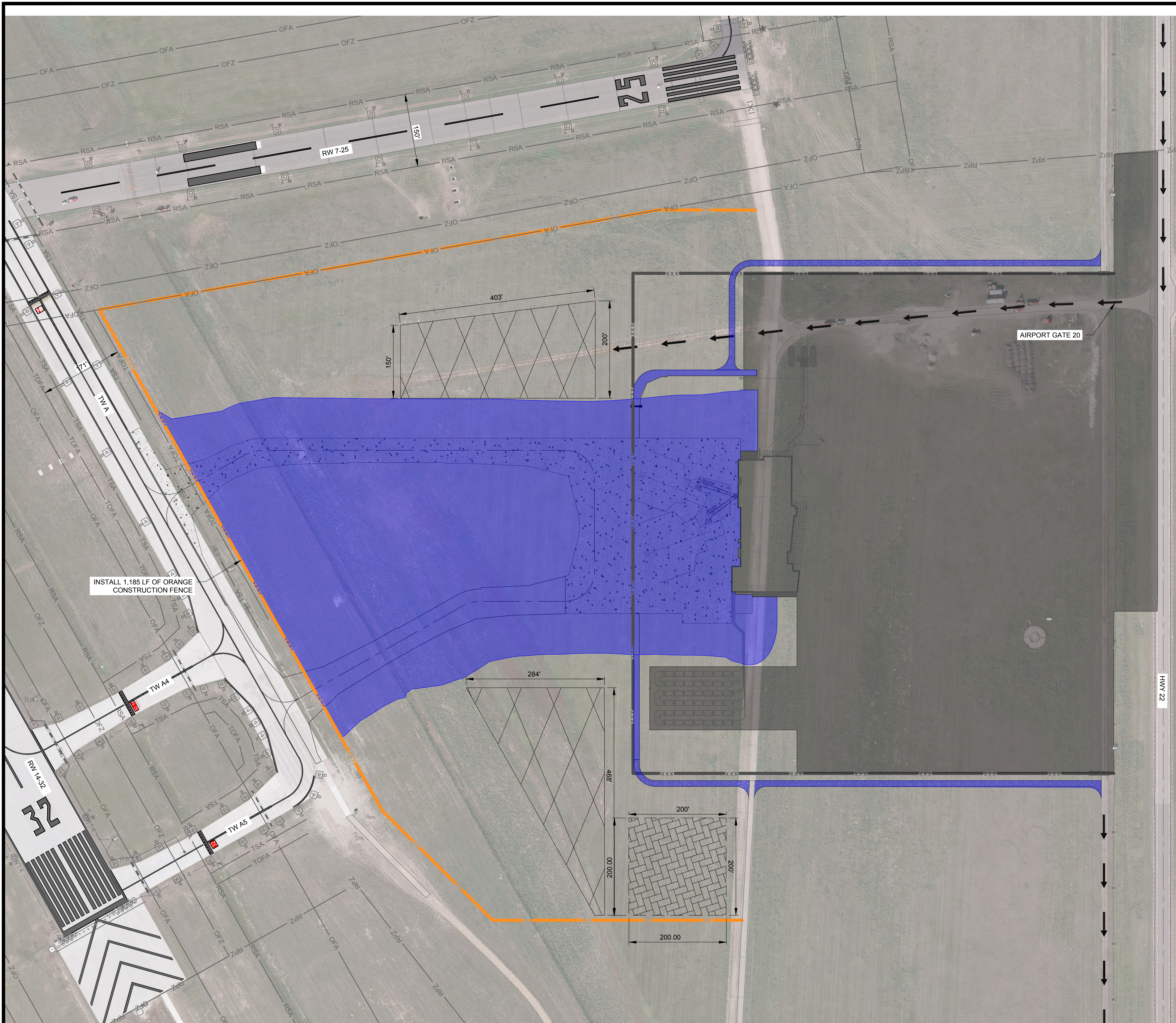
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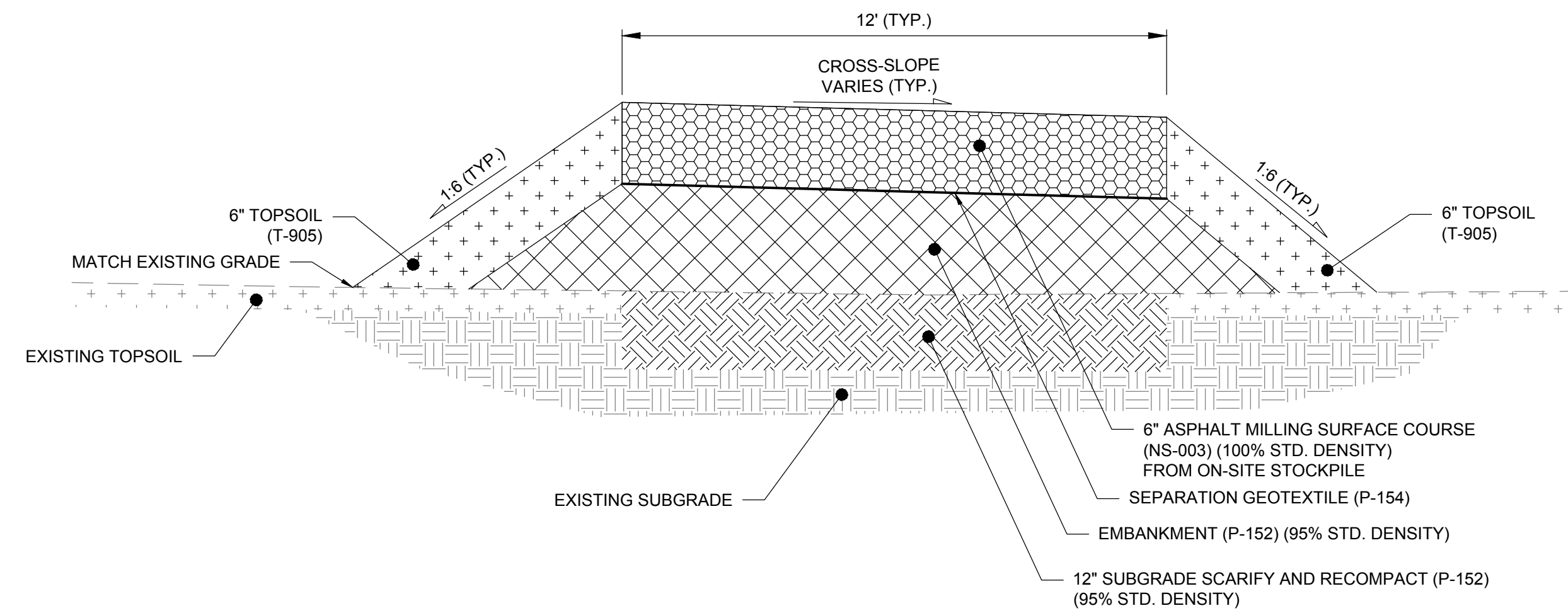
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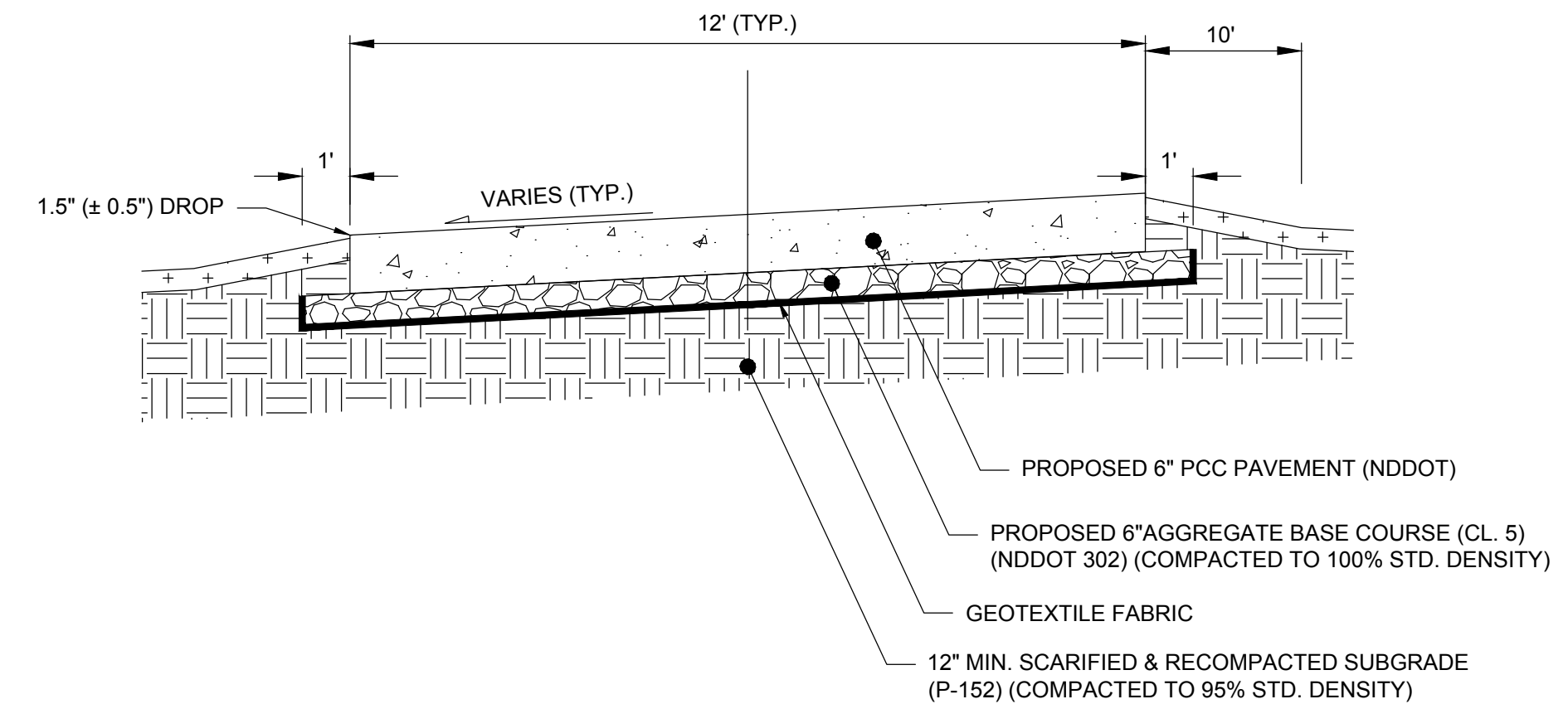
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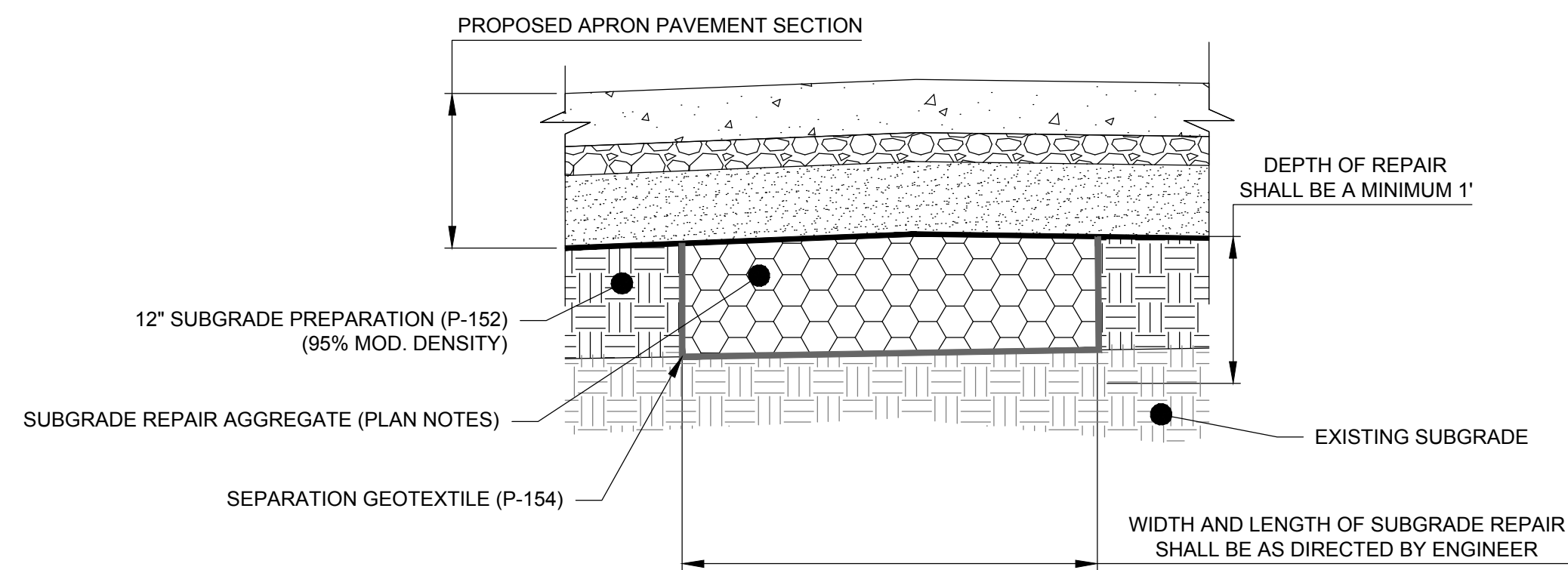
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1 PROPOSED COMPACTED MILLINGS PERIMETER ROAD TYPICAL SECTION
NOT TO SCALE



2 PROPOSED CONCRETE PERIMETER ROAD TYPICAL SECTION
NOT TO SCALE



3 SUBGRADE REPAIR DETAIL
NOT TO SCALE

SUBGRADE REPAIR NOTES:

SHOULD AN AREA OF SUBGRADE BE IDENTIFIED AS UNSUITABLE BY THE ENGINEER, THE DELINEATED AREA SHOULD BE OVEREXCAVATED A MINIMUM OF 12 INCHES AND REPLACED WITH A MATERIAL MEETING THE FOLLOWING GRADATIONAL REQUIREMENTS:

SIEVE SIZE	PERCENT PASSING
6"	100%
#4	0-60%
#200	0-20%

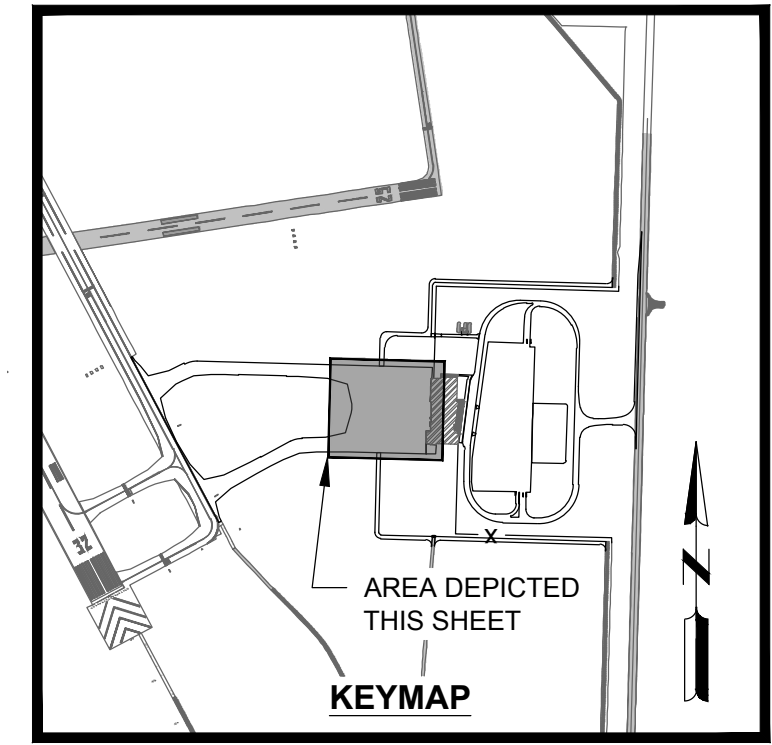
CONTRACTOR SHALL MINIMIZE EQUIPMENT TRAFFIC WHEN ACCOMPLISHING SUBGRADE SUBCUT. SUBCUT SHALL BE ACCOMPLISHED WITH SMOOTH BUCKET EXCAVATORS OR LOW-PRESSURE EQUIPMENT TO MINIMIZE IMPACT TO SUBGRADE PRIOR TO FABRIC AND AGGREGATE PLACEMENT. PROOF ROLL WITH LOW GROUND PRESSURE EQUIPMENT SHALL BE OBSERVED BY ENGINEER PRIOR TO BEGINNING FABRIC AND AGGREGATE PLACEMENT. SOFT YIELDING AREAS SHALL BE SUBCUT FURTHER AS DETERMINED BY THE ENGINEER. ADDITIONAL SOIL STABILIZATION MATERIAL AND ADDITIONAL SUBGRADE REPAIR AGGREGATE PLACED.

MEASUREMENT FOR PAYMENT SPECIFIED BY THE CUBIC YARD (CUBIC METER) SHALL BE COMPUTED BY THE THE COMPARISON OF DIGITAL TERRAIN MODEL (DTM). THE END AREA IS THAT BOUND BY THE ORIGINAL GROUND LINE ESTABLISHED BY FIELD CROSS-SECTIONS AND THE FINAL THEORETICAL PAY LINE ESTABLISHED BY CROSS-SECTIONS SHOWN ON THE PLANS, SUBJECT TO VERIFICATION BY THE ENGINEER.

PAYMENT SHALL BE MADE AT THE RESPECTIVE CONTRACT UNIT PRICE FOR "SUBGRADE REPAIR AGGREGATE" MEASURED PER CUBIC YARD INSTALLED AND ACCEPTED. THIS PRICE SHALL BE FULL COMPENSATION FOR ALL LABOR, EQUIPMENT, TOOLS SITE RESTORATION AND MATERIALS NECESSARY FOR A COMPLETE INSTALLATION OF THE ITEM.

THE SEPARATION GEOTEXTILE SHOULD BE FULLY WRAPPED IN A NON-WOVEN GEOTEXTILE FABRIC TO PREVENT THE MIGRATION OF FINES INTO THE STABILIZATION LAYER.

EQUIPMENT WILL NOT BE ALLOWED UNTIL THE FIRST LIFT OF GRANULAR MATERIAL IS IN PLACE. ALL SEAMS WILL BE OVERLAPPED AT LEAST 2'.



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CONSTRUCTION**
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ISSUED
1 06/03/26 ADDENDUM 1



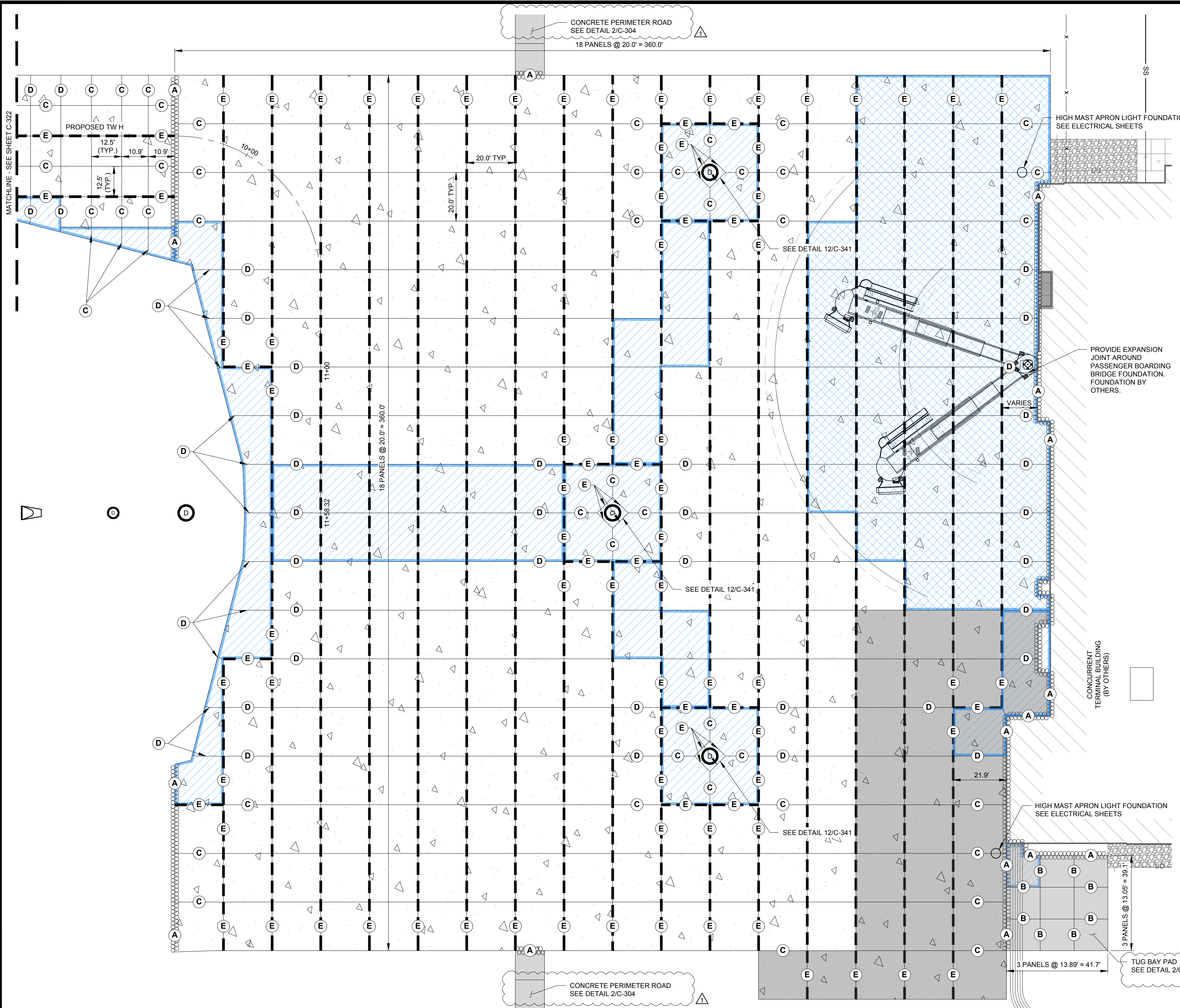
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SHEET CONTENTS
JOINTING PLAN - BASE
BID

SHEET NO.

C-321R

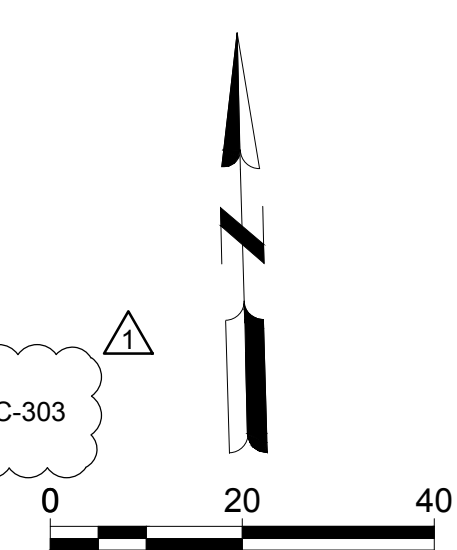


LEGEND:

- EXISTING CONCRETE PAVEMENT
- PROPOSED 11" CONCRETE PAVEMENT
- CONCRETE PAVEMENT WITH REINFORCEMENT NO. 4 REBAR 12" O.C.E.W. (SEE DETAIL 6, SHEET C-341)
- CONCRETE PAVEMENT WITH REINFORCEMENT NO. 8 REBAR 12" O.C.E.W. (SEE DETAIL 6, SHEET C-341)
- PROPOSED 8" CONCRETE PAVEMENT
- PROPOSED 6" CONCRETE PAVEMENT
- ASSUMED PAVING LANE
- THICKENED EDGE
- TYPE A THICKENED EDGE ISOLATION JOINT
- TYPE C DOWELED CONTRACTION JOINT
- TYPE D DUMMY CONTRACTION JOINT
- TYPE E DOWELED CONSTRUCTION JOINT

NOTES

1. THIS SHEET ONLY APPLIES IF BID ALTERNATE 1 IS NOT AWARDED. IF BID ALTERNATE 1 IS AWARDED, SEE SHEET C-323.
2. SEE SHEET C-341 FOR JOINTING DETAILS.
3. WITH ENGINEER'S APPROVAL, CONTRACTOR MAY MODIFY JOINTING TO ACCOMMODATE CONSTRUCTION.
4. LAYOUT ASSUMES CONCURRENT CONTRACTORS TERMINAL BUILDING IS IN PLACE. COORDINATE WITH OWNER AND ENGINEER FOR TERMINAL CONSTRUCTION PROGRESS AND ANY REVISIONS NECESSARY TO ACCOMMODATE CONSTRUCTION.
5. TERMINAL BUILDING CONTRACTOR IS SCHEDULED TO BEGIN CONSTRUCTION IN APRIL 2026 WITH COMPLETION IN NOVEMBER 2027. PRELIMINARY SCHEDULES INDICATE THE TERMINAL BUILDING STRUCTURE WILL BE ERECT AND ENCLOSED BY DECEMBER 2026.



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Bidder Questions Answered by the Engineer Up to the Issuance of Addendum No. 1

1. Question:

Are the Buy American forms, pages 2-31 through 2-46, required at the time of bid? Is the subcontractor list, page 2-47, required at the time of bid?

Response:

Pages 2-31 to 2-46 only need to be filled out if you are applying for a Type 3 Waiver. See the Buy American Certification Form on pages 2-27 to 2-29 for more information. Typically, we don't anticipate needing a Type 3 Waiver for any of the civil materials. For airfield electrical, there is a list of equipment with pre-approved waivers. On page 2-45 you will list the equipment with those pre-approved waivers that you are planning to use any. The electricians should know those if you are doing a combined bid. I'm not sure if a waiver is needed for any electrical equipment.

The subcontractor/material supplier list is not required at the time of the bid opening but would be preferred to be included and filled out as much as you can.

2. Question:

It is my understanding that access will be through Gate 20 and that there will also be a gate through the terminal building construction fence to the airfield. The notes in the CSPP require a gate guard when the terminal construction fence is not continuous. I assume this is referring to times when we would be working in between the terminal construction fence and terminal building. Does an open terminal construction fence gate constitute a non-continuous fence? When we are hauling, will we be required to guard both gates?

Response:

As the site is configured now, only the temporary gate you install in the terminal building contractor's construction fence will need a gate guard when it being left open. When their temporary construction fence is removed and the proposed security and wildlife fences to be installed with this project have not been installed yet there needs to be a gate guard at airport Gate 20. When the project gets to that point you will need to coordinate with the terminal building contractor who will be providing a gate guard on a given date. Ideally it would be who is doing the hauling that day.

3. Question:

Who is responsible for removing the terminal fence to complete our work on the apron and perimeter roads? Does it need to be reset to the configuration shown in the plans? When and where is the permanent fence being installed?

Response:

The "Concurrent Terminal Building Construction Fence" will be removed by the Terminal Building Contractor (JE Dunn Construction) once they have the building fully enclosed by the end of this year (2026). The permanent security fence can be installed once they have the building fully enclosed. Though we are anticipating it be one of the last items of the project.

4. Question:

When is the terminal contractor scheduled to be done working on the exterior of the building and have the area cleaned up so our work can be done on the west side of the terminal? I saw a note indicating that the terminal would be enclosed by the end of 2026. Does that mean that all exterior work will be done?

Response:

Like above and stated on the plans, the building is scheduled to be enclosed by the end of 2026. It is assumed some exterior work will still need to be completed as well as the installation of the passenger boarding bridges, which will require the apron to be constructed first.

5. Question:

Who is responsible for maintaining access through the terminal site?

Response:

All parties to include coordination with the apron contractor, terminal building contractor and the parking lot contractor.

6. Question:

The terminal contractor and the apron contractor (and the parking lot/loop road contractor?) are using the perimeter road to reach the waste site. Who is responsible for maintaining and restoring the perimeter road when the hauling is done? Will all the waste generated on these projects fit in the waste site? Do you anticipate there being excess needing to be exported?

Response:

Apron contractor will be responsible for maintaining and restoring the perimeter road for your hauling operations. The projects should be timed/sequenced where the Terminal building contractor has completed their hauling to the waste site by the time you start construction, and you should complete your hauling to the waste site by the time the Parking Lot/Loop Road contractor starts their work.

The waste site grading plan has been designed to accommodate the planned amount of waste material. If there is excess waste material, we will adjust the waste area grading plan rather than you export the material off site.

7. Question:

Who is responsible for shaping and seeding the waste pile? Is the seeding paid for? Please provide a plan sheet showing the area paid for under the seeding bid item?

Response:

Apron contractor will be responsible for shaping and seeding the waste area. The terminal building contractor will be using the area this summer for their waste material and will most likely have restored it before you start work this fall.

The grading plan for the waste area is shown on C-107. The seeding of the waste area is included in the seeding quantity. Sheets C-021 to C-024 show the areas included in the seeding & mulching quantity. Restoration outside the limits shown on these sheets shall be the responsibility of the contractor.

8. Question:

Is there an on-site source for construction water?

Response:

No.

9. Question:

The note at the top of Sheets C-321 and C323 points to detail 2/C-303. I assume that should that be 2/C-304?

Response:

Correct.

10. Question:

Where on-site is the stockpile of millings to be used for the perimeter road? The plans notes indicate that there should be more than enough millings for the intended purpose. However, if there is insufficient millings to build the perimeter road, the remaining portion will be constructed using class 5. If there is less millings than stated, who is responsible for added cost of the class?

Response:

Sheets C-083, C-111, and C-112 show the existing asphalt stockpile locations. They are located on the north side of the future loop road. The additional Class 5 aggregate will be paid under the "Aggregate Base Course, Class 5" line item, if the amount needed exceeds 15% of that quantity a change order will be required.

11. Question:

The quantity for NDDOT 550, 6" Non-Reinforced Concrete isn't working out. The plan quantity = 2,487 SY. Sheet C-325 = 759 SY. Are we missing a page somewhere?

Response:

There is an error in that quantity and will be fixed with an addendum. The actual quantity of NDDOT 6" plain PCC is 764 SY.

12. Question:

Will the boarding bridge foundation be installed before or after the pavement?

Response:

Before, the passenger boarding bridge foundations will be installed with the terminal building foundations.

13. Question:

On the subject project what type of pipe is required for the 4" Perforated Underdrain Pipe? The bid form says polyethylene and Sch 40. Should it be the single wall perforated corrugated poly or Sch 40 Perforated PVC?

Response:

It should be single wall perforated corrugated poly. An addendum will be issued today or tomorrow addressing a few other questions and will provide an updated bid form correcting the item description.

If a smooth-wall perforated PVC is preferred an acceptable substitution would need to meet the requirements of Paragraph 705-2.2 of the Specifications.

14. Question:

Can they replace the bid schedule with their own generated with excel? Will you provide the excel spread sheet?

Response:

Yes, you can use your own generated bid schedule. Yes, a Excel spreadsheet will be provided by request.

15. Question:

Can you provide an estimated cost?

Response:

Bid Schedule 1 - Base Bid (General / Civil): \$6.25 - \$6.75 million

Bid Alternate 1A Bid Alternate No. 1 (General / Civil): \$1.0 - \$1.5 million

Bid Schedule 2 - Base Bid (Electrical): \$500k - \$600k

Bid Alternate 1B Bid Alternate No. 1 (Electrical): \$200k - \$300k

Bid Schedule 3 - Base Bid (Combined): \$7.0 - \$7.5 million

Bid Alternate 1C Bid Alternate No. 1 (Combined): \$1.25 - \$1.75 million.

16. Question:

Also wanted to clarify the Liquidated Damages for this project. In Section 80 it states that they are \$2,800.00, but in the contract, it states that they are \$3,300.00 – do you know which one is correct?

Response:

Addendum No. 1 will address this conflict with an update to the Specifications.

17. Question:

On the Bid Form do we need to write in words and numbers or will just numbers be ok?

Response:

Just writing numbers on the Bid Form is acceptable.

18. Question:

Is aviation exposure insurance required?

Response:

Most Umbrella coverages do not cover damage to aircraft. This Project is on an active Airport and parts of the Project are next to the open runway and taxiway.

19. Question:

Can all of the concrete pavement be placed with fixed forms?

Response:

Yes, the total volume of concrete on the project is below the threshold listed in Specification Section 501-4.2b(2) allowing fixed form placement. You still need to follow the vibration requirement in Section 501-4.2c, handheld vibrators may only be used in irregular areas.

20. Question:

I see the RCP Storm Trench Detail calls for P-154 from the spring-line of pipe to the base gravel with 100% of a modified compaction. Is that truly what is needed, as it says 95% in the Specifications?

Response:

You are correct, that overfill area between the spring line, and the bottom of subgrade should be 95% compaction of the Modified Proctor.

21. Question:

I did note see a trench detail for the CMP or PVC piping. Is it the same as RCP?

Response:

CMP and PVC pipe will be the same as the RCP.